

03 - STRUCTURES  
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GENERAL NOTES

1. FOR BRIDGE LOCATIONS SEE HIGHWAY PLANS.
2. FOR ALL BRIDGE PARAPET JOINT DETAILS SEE DRAWING NO. S-9, DRAWING TITLE: STAGE CONSTRUCTION JOINT IN PAVEMENT CUTOUT.
3. THE DEPTH OF PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM IS ESTIMATED TO BE ON AVERAGE,  $\frac{3}{4}$ ".
4. USE ASPHALTIC PLUG JOINT SYSTEM FOR ABUTMENTS NOS. 1 AND 2 OF BRIDGE NO. 01770
5. DUE TO THE DIFFERENTIAL DEPTH BETWEEN ROADWAY AND BRIDGE ASPHALT MILLING AND PRIOR TO MILLING OPERATIONS THE CONTRACTOR SHALL VERIFY THE ENDS OF CONCRETE BRIDGE DECKS AND BACKWALL AND ADJUST MILLING OPERATIONS TO PROTECT THE CONCRETE FROM DAMAGE.
6. JOINT THREATMENT SHALL BE PERFORMED ONLY AT SOUTHBOUND FOR THE BRIDGE NO. 01639.

PREFORMED SILICONE JOINT SEAL NOTES

1. PRIOR TO INSTALLING THE SILICONE SEALANT, CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK SHALL BE PAID FOR UNDER THE ITEM "PREFORMED SILICONE JOINT SEALING SYSTEM".
2. ALL PREFORMED SILICONE JOINT SEAL SHALL BE SILICOFLEX SF-400 WITH THE EXCEPTION OF THE FOLLOWING JOINTS:

BRIDGE NO. 01770: PIER NOS. 1 AND 2

THE DESIGN APPEARS TO CONFORM TO APPLICABLE CRITERIA. APPROVAL IS NOT TO BE CONSTRUED TO MEAN THAT ALL ASPECTS OF THE DESIGN HAVE BEEN PERSONALLY CHECKED BY THE UNDERSIGNED.

TRANSPORTATION PRINCIPAL ENGINEER

GENERAL NOTES

SPECIFICATIONS: CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 816 (2004), SUPPLEMENTAL SPECIFICATIONS DATED JULY 2013 AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS CUSTOMARY U.S. UNITS 6TH EDITION - 2012, INCLUDING 2013 INTERM REVISIONS AS SUPPLEMENTED BY THE CONNECTICUT DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL (2003)



REINFORCING BARS: REINFORCING BARS IN ALL CONCRETE RECONSTRUCTIONS SHALL CONFORM TO ASTM A615, GRADE 60, AND BE GALVANIZED IN ACCORDANCE WITH ASTM A767, CLASS 1.

ASPHALTIC PLUG EXPANSION  
JOINT SYSTEM NOTES

1. A BRIDGING PLATE SHALL BE USED TO SPAN THE GAP BETWEEN TWO DECK ENDS OR THE JOINT BETWEEN A DECK END AND A CONCRETE BACKWALL.
2. DISCONTINUE THE INSTALLATION OF THE BACKER ROD, BRIDGING PLATE AND LOCATING PIN WHERE THE APPROACH SLAB IS DISCONTINUED (TYPICALLY IN THE ROADWAY SHOULDERS). SEE ASPHALTIC PLUG EXPANSION JOINT SYSTEM SPECIAL PROVISION.
3. NEW STEEL BRIDGING PLATES SHALL HAVE A MINIMUM THICKNESS OF  $\frac{1}{4}$ ". FOR JOINT OPENINGS THAT EXCEED 3" A  $\frac{3}{8}$ " THICK BY 12" WIDE PLATE WILL BE REQUIRED.
4. NO BRIDGING PLATE SHALL BE USED AT THE FOLLOWING LOCATIONS:

A. JOINT BETWEEN A DECK END AND A CONCRETE APPROACH PAVEMENT

B. WHERE A BRIDGE DECK END MEETS A BITUMINOUS APPROACH PAVEMENT
5. SAW-CUTS MADE 3' EACH SIDE OF CENTERLINE OF JOINT WILL BE PAID AS "CUT BITUMINOUS CONCRETE PAVEMENT".
6. THE REMOVAL OF ALL EXISTING JOINT SYSTEMS AND BITUMINOUS CONCRETE WITHIN THE LIMITS SHOWN TO BE INCLUDED FOR PAYMENT UNDER THE ITEM "REMOVAL OF HMA WEARING SURFACE".
7. INSTALLATION OF MEMBRANE WITHIN THE LIMITS SHOWN TO BE PAID UNDER THE ITEM, "MEMBRANE WATERPROOFING (SHEET) (TORCH-APPLIED)"
8. CRACK SEALANT PLACED ALONG VERTICAL FACES OF THE SAW-CUT PAVEMENT TO BE PAID UNDER THE ITEM, "JOINT AND CRACK SEALING OF BITUMINOUS CONCRETE PAVEMENT".
9. THE FURNISHING AND PLACING OF HMA S0.375 TO BE INCLUDED FOR PAYMENT UNDER THE ITEM "HMA S0.375".
10. SAW-CUTTING AND REMOVAL OF PAVEMENT FOR JOINT INSTALLATION TO BE INCLUDED UNDER FOR PAYMENT THE ITEM, "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".
11. CLOSED CELL BACKER ROD DIAMETER SHALL BE DETERMINED AFTER MEASURING THE JOINT OPENING. THE ROD SHALL BE 25% LARGER THAN THE JOINT OPENING.
12. ASPHALTIC PLUG EXPANSION JOINT SYSTEMS MAY BE INSTALLED ONLY WITHIN THE TEMPERATURE RANGE SPECIFIED IN THE SPECIAL PROVISION "ASPHALTIC PLUG EXPANSION JOINT SYSTEM". REFERENCE THE RANGE OF THERMAL MOVEMENT FOR THE SELECTED JOINT PRODUCT IN THE TABLE FOR "ALLOWABLE BRIDGE SUPERSTRUCTURE SURFACE TEMPERATURE RANGE" IN THE SPECIAL PROVISION.

-	-	-	-	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	DESIGNER/DRAFTER: <b>BU</b>	 <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b>	SIGNATURE/ BLOCK: <b>OFFICE OF ENGINEERING</b>  APPROVED BY: 	PROJECT TITLE:  <b>PAVEMENT PRESERVATION ON I-95</b>	TOWN:  <b>TOWN OF GROTON</b>	PROJECT NO. <b>58-329</b>
-	-	-	-		CHECKED BY: <b>RPL</b>					DRAWING NO. <b>S-1</b>
-	-	-	-		SCALE AS NOTED					SHEET NO. <b>04.01</b>
REV.	DATE	REVISION DESCRIPTION	SHEET NO.		Plotted Date: 10/17/2013	Filename: ...\\SB_MSH_S-1_BRIDGE_DESIGN_INDEX_OF_DRAWINGS.dgn				

BRIDGE INFORMATION FOR THE INSTALLATION OF BRIDGE DECK JOINTS (TOWN OF GROTON)											
BRIDGE NO.	BRIDGE DATA							BRIDGE JOINT LOCATIONS			
	FINE MILLING DEPTH (IN)	HMA S0.5 DEPTH (IN)	MILE POINT	LOCATION	NO. OF TRAVEL LANES	* CURB TO CURB WIDTH (FT)	* SKEW (DEG)	ABUTMENT NO. 1	ABUTMENT NO. 2		
01769	1.5"	2"	94.93	I-95 SOUTHBOUND OVER I-95 RAMP 313,	4	80.0	34.0				
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.82		
DECK JOINT DETAIL								B/S-6	B/S-6		
PARAPET JOINT DETAIL								F/S-10	F/S-10		
BRIDGE NO.	FINE MILLING DEPTH (IN)	HMA S0.5 DEPTH (IN)	MILE POINT	LOCATION	NO. OF TRAVEL LANES	* CURB TO CURB WIDTH (FT)	** SKEW (DEG)	ABUTMENT NO. 1	PIER NO. 1	PIER NO. 2	ABUTMENT NO. 2
01639	1"	2"	94.80	I-95 RAMP 313 OVER AMTRAK	2	35	19				
MAXIMUM THERMAL EXPANSION (IN.)				GROTON - SOUTHBOUND ONLY				0.00	0.85	0.99	0.37
DECK JOINT DETAIL								B/S-6	N/A	N/A	B/S-6
PARAPET JOINT DETAIL								Q/S-14	N/A	N/A	Q/S-14
BRIDGE NO.	FINE MILLING DEPTH (IN)	HMA S0.5 DEPTH (IN)	MILE POINT	LOCATION	NO. OF TRAVEL LANES	* CURB TO CURB WIDTH (FT)	** SKEW (DEG)	ABUTMENT NO. 1	ABUTMENT NO. 2		
02445	1.5"	2"	94.93	I-95 NOURTHBOUND OVER I-95 RAMP 313	7	148.0	3.0				
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.87		
DECK JOINT DETAIL								B/S-6	B/S-6		
PARAPET JOINT DETAIL								O/S-13	O/S-13		
BRIDGE NO.	FINE MILLING DEPTH (IN)	HMA S0.5 DEPTH (IN)	MILE POINT	LOCATION	NO. OF TRAVEL LANES	* CURB TO CURB WIDTH (FT)	** SKEW (DEG)	ABUTMENT NO. 1	PIER NO. 1	PIER NO. 2	ABUTMENT NO. 2
01770	1.5"	2"	95.19	I-95 SB OVER ROUTE 184 & I-95 TR827,	3	50.0	57.0				
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.65	1.01	0.99
DECK JOINT DETAIL								E/S-8	C/S-7	C/S-7	E/S-8
PARAPET JOINT DETAIL								H/S-11&S-12	S/S-15	S/S-15	H/S-11&S-12
BRIDGE NO.	FINE MILLING DEPTH (IN)	HMA S0.5 DEPTH (IN)	MILE POINT	LOCATION	NO. OF TRAVEL LANES	* CURB TO CURB WIDTH (FT)	** SKEW (DEG)	ABUTMENT NO. 1	PIER NO. 1	PIER NO. 2	ABUTMENT NO. 2
01771	1.5"	2"	95.30	I-95 SB OVER ROUTE 12,	3	50.0	44.0				
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.66	1.04	0.67	0.00
DECK JOINT DETAIL								B/S-6	A/S-5	A/S-5	B/S-6
PARAPET JOINT DETAIL								F/S-10	S/S-15	S/S-15	F/S-10
BRIDGE NO.	FINE MILLING DEPTH (IN)	HMA S0.5 DEPTH (IN)	MILE POINT	LOCATION	NO. OF TRAVEL LANES	* CURB TO CURB WIDTH (FT)	** SKEW (DEG)	ABUTMENT NO. 1	PIER NO. 1	PIER NO. 2	ABUTMENT NO. 2
01772	1.5"	2"	95.33	I-95 NB OVER ROUTE 12,	1	30.0	36.0				
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.0	0.27	0.92	0.27
DECK JOINT DETAIL								B/S-6	A/S-5	A/S-5	B/S-6
PARAPET JOINT DETAIL								F/S-10	S/S-15	S/S-15	F/S-10
BRIDGE NO.	FINE MILLING DEPTH (IN)	HMA S0.5 DEPTH (IN)	MILE POINT	LOCATION	NO. OF TRAVEL LANES	* CURB TO CURB WIDTH (FT)	** SKEW (DEG)	ABUTMENT NO. 1	ABUTMENT NO. 2		
01773	1.5"	2"	95.40	I-95 NB OVER SR900 (BONNIE CIRCLE),	3	50.0	7.0				
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.85	0.00		
DECK JOINT DETAIL								B/S-6	B/S-6		
PARAPET JOINT DETAIL								F/S-10	F/S-10		

\* MEASURED PERPENDICULAR TO CENTERLINE ROADWAY.  
FOR BRIDGE NO. 02445, THE MEDIAN WIDTH IS INCLUDED  
TO THE TOTAL BRIDGE WIDTH.



\*\* MEASURED FROM A LINE PERPENDICULAR TO CENTERLINE OF ROADWAY.

[illegible]

BRIDGE INFORMATION FOR THE INSTALLATION OF BRIDGE DECK JOINTS (TOWN OF GROTON)																		
BRIDGE NO.	BRIDGE DATA							BRIDGE JOINT LOCATIONS										
	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	*CURB TO CURB	**SKEW	ABUTMENT	ABUTMENT									
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 2									
01774	1.5"	2"	95.41	I-95 SB OVER SR900 (BONNIE CIRCLE),	3	50.0	31.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				1.02	0.00									
DECK JOINT DETAIL								B/S-6	B/S-6									
PARAPET JOINT DETAIL								F/S-10	F/S-10									
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	*CURB TO CURB	**SKEW	ABUTMENT	PIER	PIER	ABUTMENT							
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 2							
01775	1.5"	1.5"	96.66	I-95 SB OVER BUDDINGTON ROAD,	3	51.8	16.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.29	0.59	0.29	0.00							
DECK JOINT DETAIL								B/S-6	A/S-5	A/S-5	B/S-6							
PARAPET JOINT DETAIL								F/S-10	S/S-15	S/S-15	F/S-10							
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	*CURB TO CURB	**SKEW	ABUTMENT	PIER	PIER	ABUTMENT							
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 2							
01776	1.5"	1.5"	96.68	I-95 NB OVER BUDDINGTON ROAD,	3	51.8	16.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.34	0.53	0.22	0.00							
DECK JOINT DETAIL								B/S-6	A/S-5	A/S-5	B/S-6							
PARAPET JOINT DETAIL								F/S-10	S/S-15	S/S-15	F/S-10							
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	*CURB TO CURB	**SKEW	ABUTMENT	PIER	PIER	PIER	PIER	PIER	PIER	PIER	PIER	PIER	ABUTMENT
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 3	NO. 4	NO. 5	NO. 6	NO. 7	NO. 8	NO. 1	
01777	1.5"	1.5"	96.73	I-95 NB OVER BUDDINGTON ROAD,	3	50.0	0.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0	
DECK JOINT DETAIL								A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	
PARAPET JOINT DETAIL								S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	*CURB TO CURB	**SKEW	ABUTMENT	PIER	PIER	PIER	PIER	PIER	ABUTMENT				
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2	NO. 3	NO. 4	NO. 5	NO. 2				
01778	1.5"	1.5"	96.75	I-95 NB OVER GROTON RESERVOIR,	3	50.0	0.0											
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.74	0.74	0.74	0.74	0.74	0.74	0.00				
DECK JOINT DETAIL								A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5	A/S-5				
PARAPET JOINT DETAIL								S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15	S/S-15				

\* MEASURED PERPENDICULAR TO CENTERLINE ROADWAY.  
FOR BRIDGE NO. 02445, THE MEDIAN WIDTH IS INCLUDED  
TO THE TOTAL BRIDGE WIDTH.

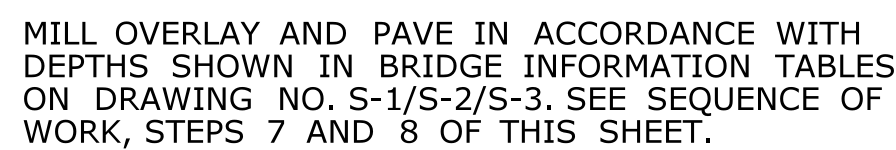
\*\* MEASURED FROM A LINE PERPENDICULAR TO CENTERLINE OF ROADWAY.


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-	-	-	-		CHECKED BY: <b>RPL</b>		APPROVED BY: 			
-	-	-	-		SCALE AS NOTED					
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		SHEET NO.		Plotted Date: 10/17/2013						

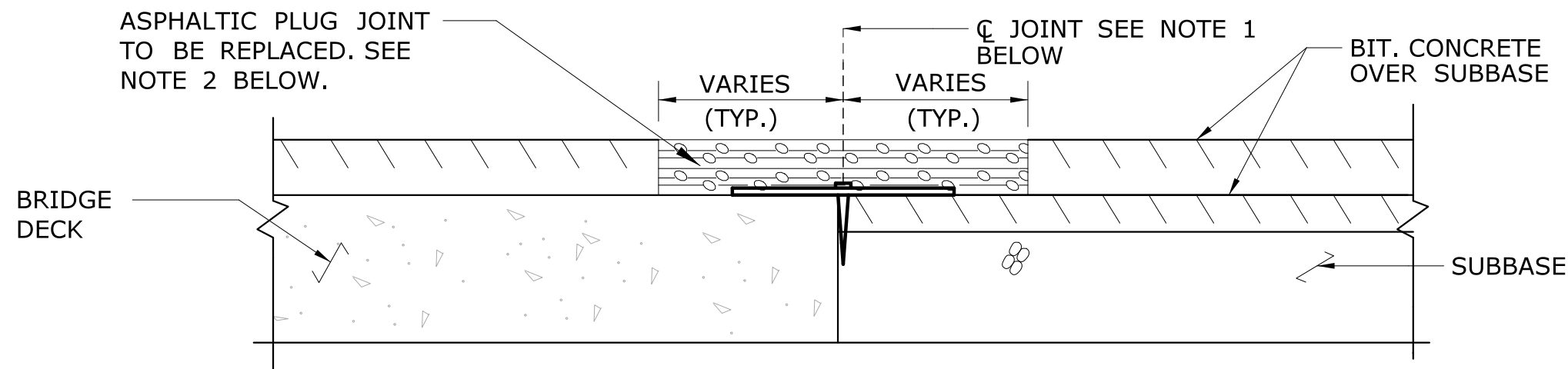
BRIDGE INFORMATION FOR THE INSTALLATION OF BRIDGE DECK JOINTS (TOWN OF GROTON)										
BRIDGE NO.	BRIDGE DATA							BRIDGE JOINT LOCATIONS		
					NO. OF TRAVEL	* CURB TO CURB	** SKEW	ABUTMENT	ABUTMENT	
	DEPTH (IN)	DEPTH (IN)	POINT	LOCATION	LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 2	
01779	1.5"	2"	97.15	I-95 NB OVER POHEGNUT POND,	3	50.0	0.0			
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.91	
DECK JOINT DETAIL								A/S-5	A/S-5	
PARAPET JOINT DETAIL								S/S-15	S/S-15	
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	* CURB TO CURB	** SKEW	ABUTMENT	ABUTMENT	
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 2	
01780	1.5"	2"	97.15	I-95 SB OVER POHEGNUT POND,	3	50.0	0.0			
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.95	
DECK JOINT DETAIL								A/S-5	A/S-5	
PARAPET JOINT DETAIL								S/S-15	S/S-15	
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	* CURB TO CURB	** SKEW	ABUTMENT	PIER	ABUTMENT
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2
01781	1.5"	2"	97.48	I-95 NB ROUTE 117,	3	50.0	8.0			
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.67	0.68
DECK JOINT DETAIL								B/S-6	A/S-5	B/S-6
PARAPET JOINT DETAIL								F/S-10	S/S-15	F/S-10
BRIDGE NO.	FINE MILLING	HMA S0.5	MILE	LOCATION	NO. OF TRAVEL	* CURB TO CURB	** SKEW	ABUTMENT	PIER	ABUTMENT
	DEPTH (IN)	DEPTH (IN)	POINT		LANES	WIDTH (FT)	(DEG)	NO. 1	NO. 1	NO. 2
01782	1.5"	2"	97.49	I-95 SB ROUTE 117,	4	60.0	9.0			
MAXIMUM THERMAL EXPANSION (IN.)				GROTON				0.00	0.68	0.68
DECK JOINT DETAIL								B/S-6	A/S-5	B/S-6
PARAPET JOINT DETAIL								F/S-10	S/S-15	F/S-10

\* MEASURED PERPENDICULAR TO CENTERLINE ROADWAY.  
FOR BRIDGE NO. 02445, THE MEDIAN WIDTH IS INCLUDED  
TO THE TOTAL BRIDGE WIDTH.

\*\* MEASURED FROM A LINE PERPENDICULAR TO CENTERLINE OF ROADWAY.



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-	-	-	-	-		CHECKED BY: <b>RPL</b>						DRAWING NO. <b>S-5</b>
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REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/17/2013		SCALE AS NOTED		Filename: ...\\SB_MSH-S-5-ASPALTIC.PLUG.EXPANSION.JOINT.DETAIL_1.dgn			DRAWING TITLE: <b>ASPHALTIC PLUG EXPANSION JT. DETAILS 1</b>	SHEET NO. <b>04.05</b>

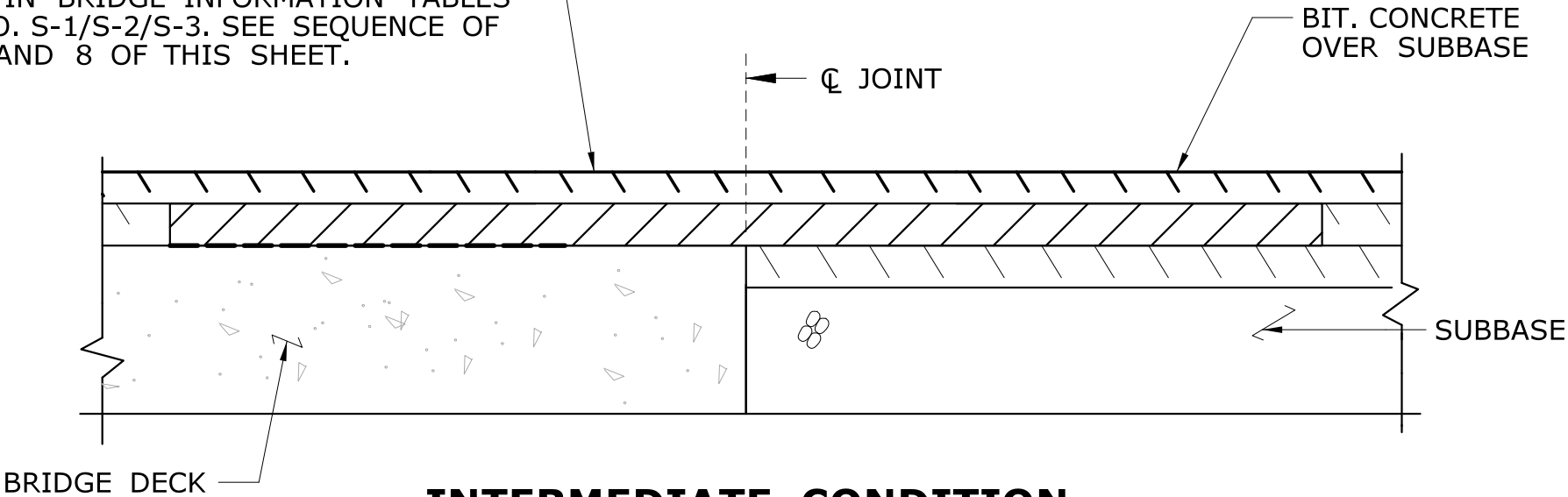


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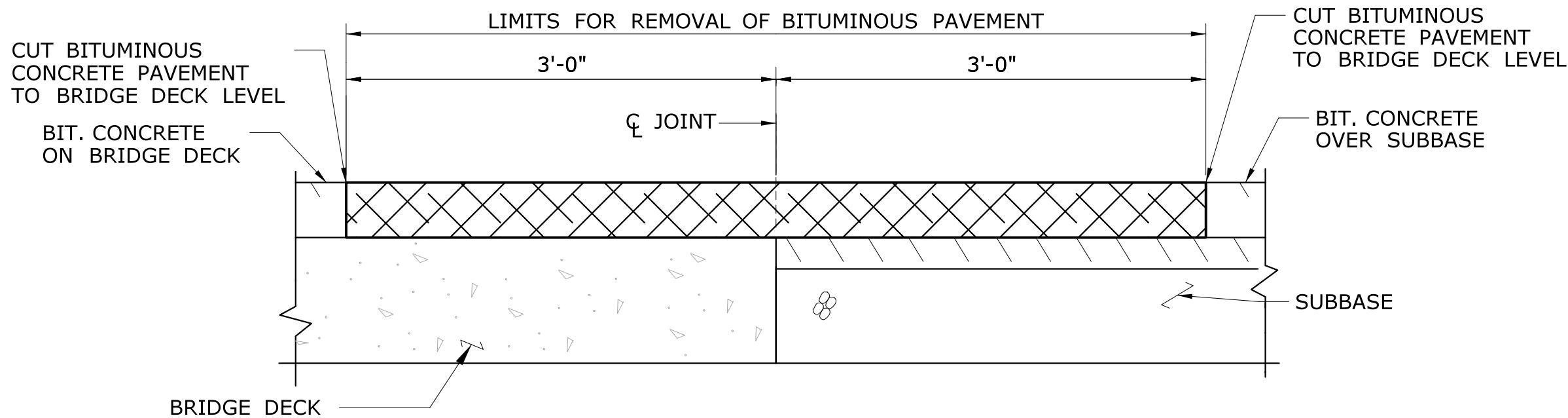
NOTES:

1. REFER TO STEP 1 OF SEQUENCE OF WORK.
2. ALL DIMENSIONS THAT REFER TO EXISTING CONSTRUCTION SHALL BE VERIFIED AT THE SITE BY THE CONTRACTOR.

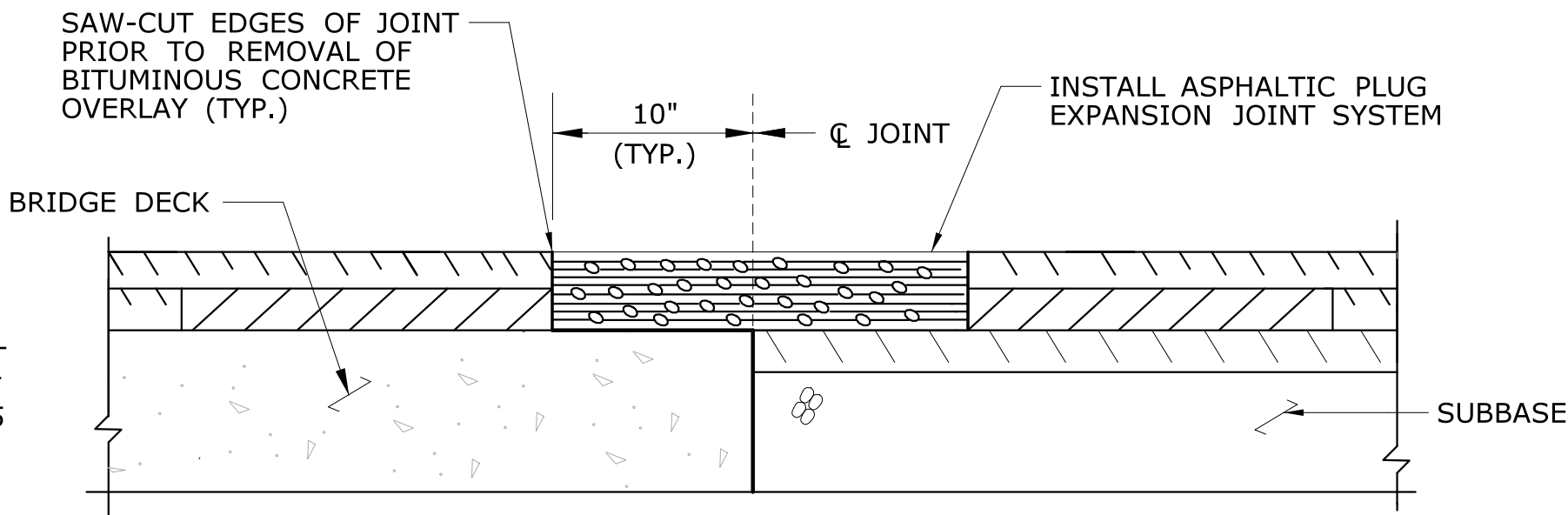
MILL OVERLAY AND PAVE IN ACCORDANCE WITH DEPTHS SHOWN IN BRIDGE INFORMATION TABLES ON DRAWING NO. S-1/S-2/S-3. SEE SEQUENCE OF WORK, STEPS 7 AND 8 OF THIS SHEET.



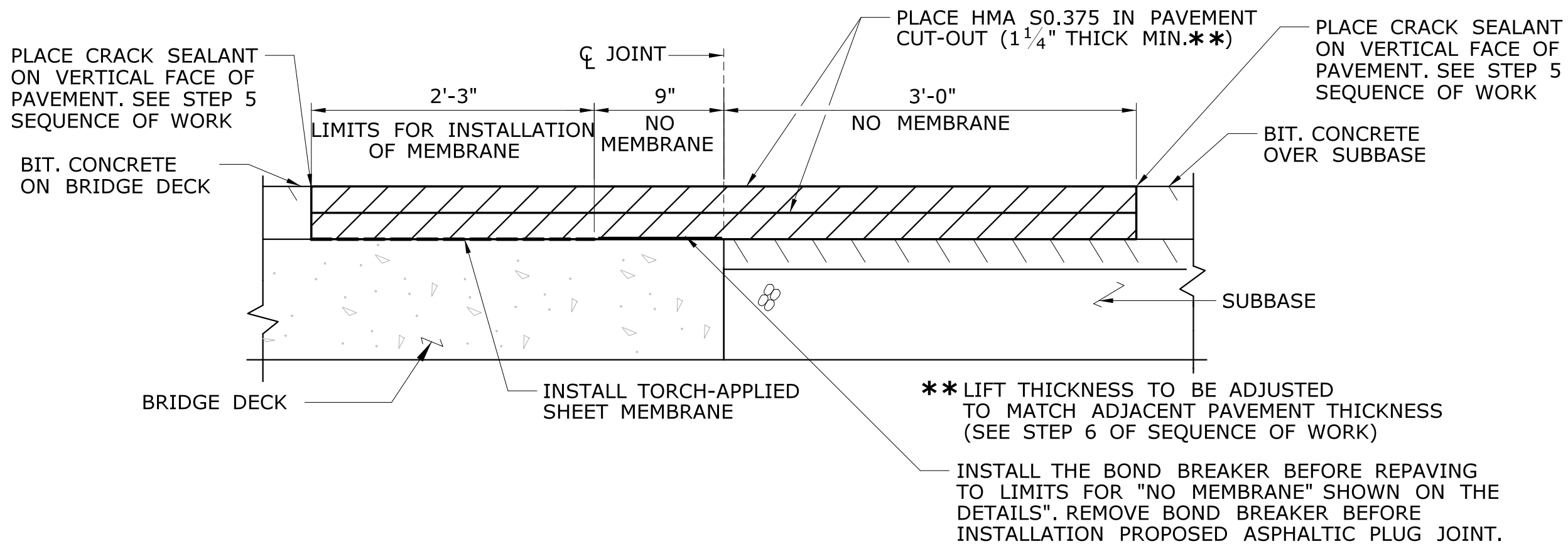
**INTERMEDIATE CONDITION  
MILLING AND PAVING (STEPS 7 & 8)**



**INTERMEDIATE CONDITION  
JOINT AND PAVEMENT REMOVAL (STEPS 1-3)**



**FINAL CONDITION (STEPS 9 & 10)**



**INTERMEDIATE CONDITION  
(STEPS 4-6)**

NOTE: SEE DRAWING S-9 FOR STAGE CONSTRUCTION JOINT IN PARAPET CUTOUT

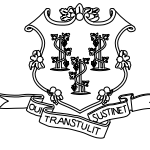

**PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM  
WITHOUT BRIDGING PLATE**

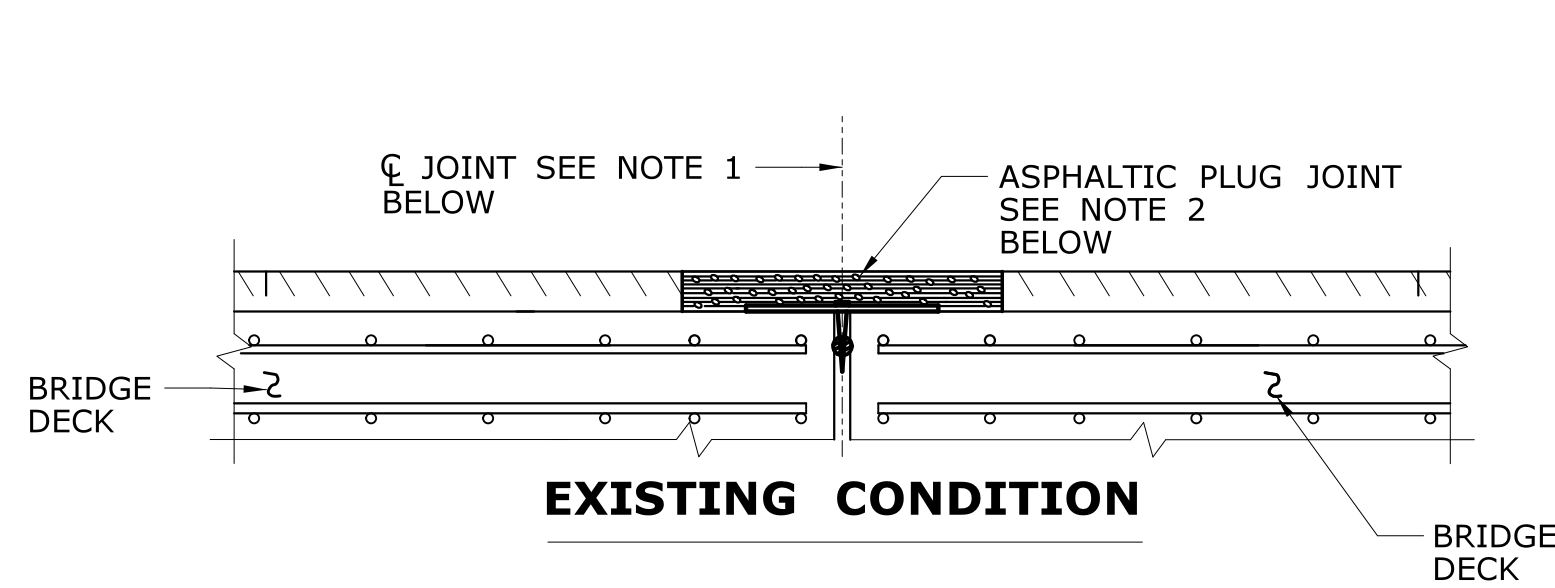
N.T.S.

**B**

**SUGGESTED SEQUENCE OF WORK**

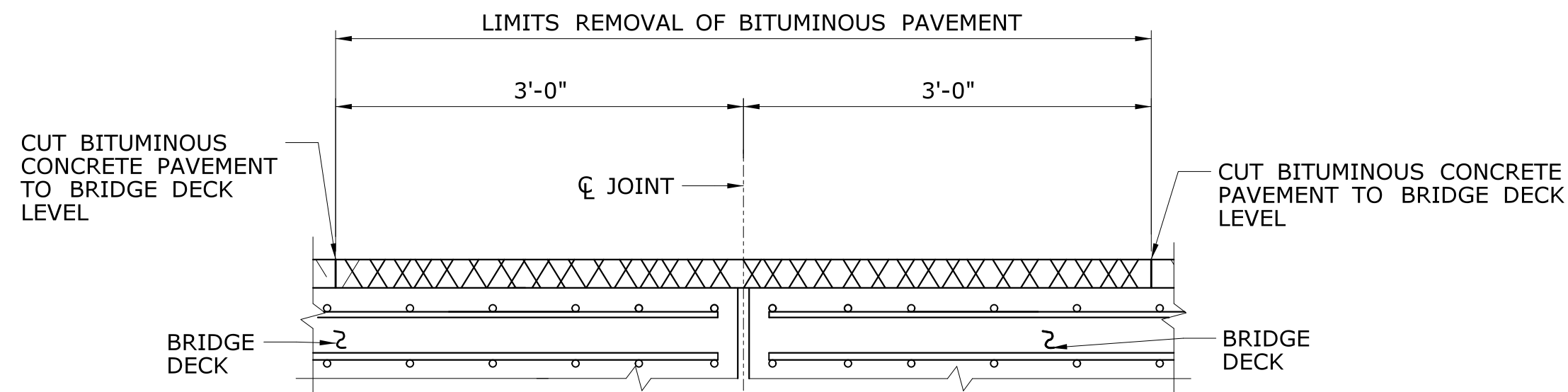
- STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.
- STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT CUT-OUT. EACH SAW CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE EXISTING DECK.
- STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT MATERIAL (BINDER, BRIDGING PLATE, LOCATING PIN AND BACKER ROD) WITHIN THE LIMITS SHOWN.
- STEP 4: INSTALL MEMBRANE TO THE TOP OF DECK WITHIN THE LIMITS SHOWN. INSTALL THE BOND BREAKER BEFORE REPAVING TO LIMITS FOR "NO MEMBRANE" SHOWN ON THE DETAILS".
- STEP 5: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES.
- STEP 6: PLACE HMA S0.375 IN THE JOINT CUTOUT. (REFER TO NOTICE TO CONTRACTOR - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS)
- STEP 7: MILL ROADWAY AND BRIDGE PAVEMENT TO SPECIFIED DEPTHS.
- STEP 8: PAVE TOP COURSE ON ROADWAY AND BRIDGE.
- STEP 9: CUT PAVEMENT FULL DEPTH, 10" EACH SIDE OF CENTER OF JOINT, AND REMOVE ALL PAVEMENT MATERIAL BETWEEN SAW-CUTS AND THE BOND BREAKER.
- STEP 10: INSTALL PROPOSED ASPHALTIC PLUG EXPANSION JOINT SYSTEM.

- - -			-	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	DESIGNER/DRAFTER: <b>BU</b>	 <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b>	SIGNATURE/ BLOCK: <b>OFFICE OF ENGINEERING</b>	<b>PAVEMENT PRESERVATION ON I-95</b>	TOWN: <b>TOWN OF GROTON</b>	PROJECT NO. <b>58-329</b>
-	-	-	-		CHECKED BY: <b>RPL</b>		APPROVED BY: 		DRAWING TITLE: <b>ASPHALTIC PLUG EXPANSION JT. DETAILS 2</b>	DRAWING NO. <b>S-6</b>
-	-	-	-		SCALE AS NOTED					SHEET NO. <b>04.06</b>
-	-	-	-			Filename: ...\\SB_MSH_S-6.ASPHALTIC_PLUG_EXPANSION_JOINT_DETAIL_2.dgn				
REV.	DATE	REVISION DESCRIPTION	SHEET NO.		Plotted Date: 10/17/2013					
-	-	-	-							

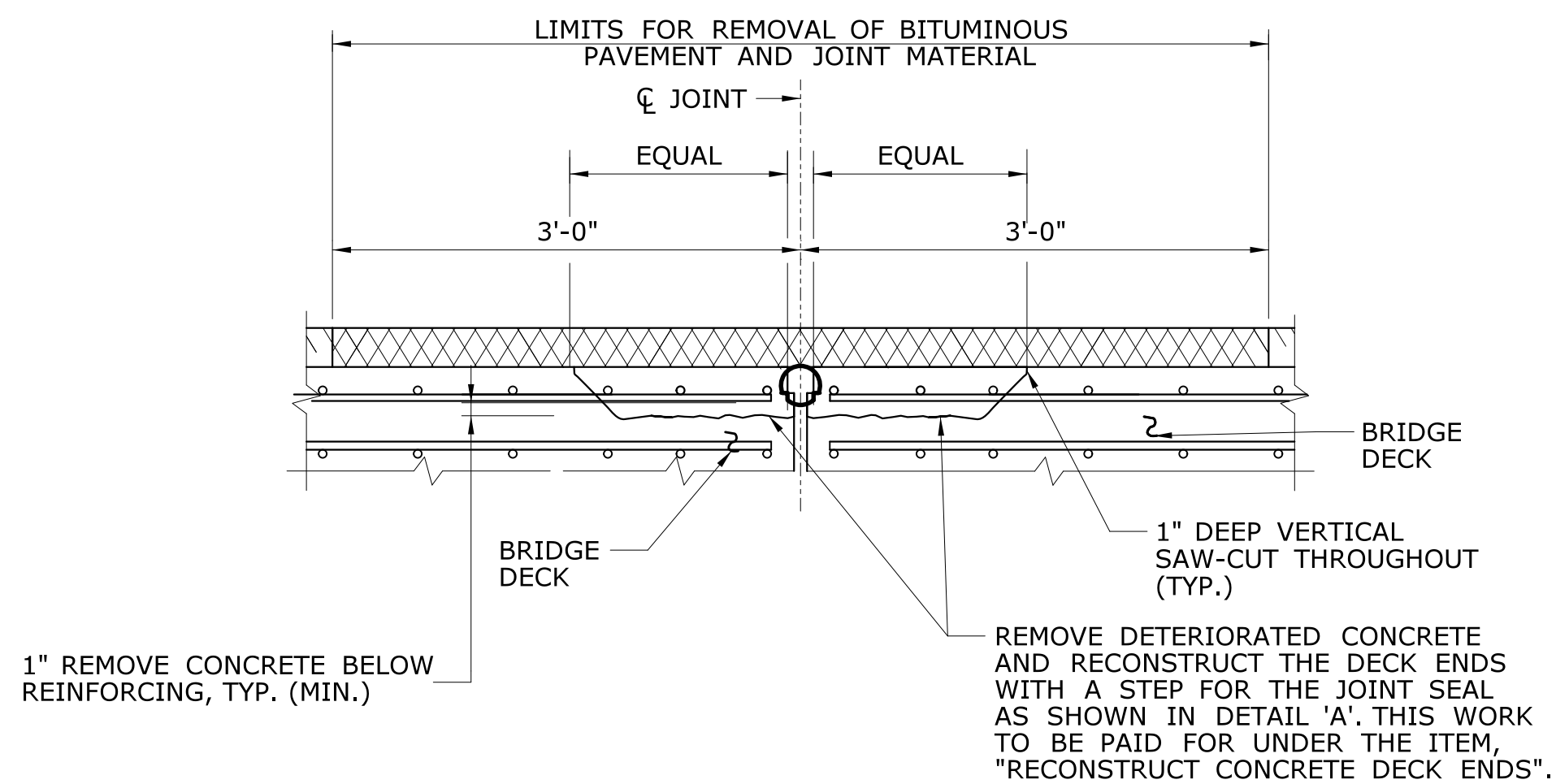


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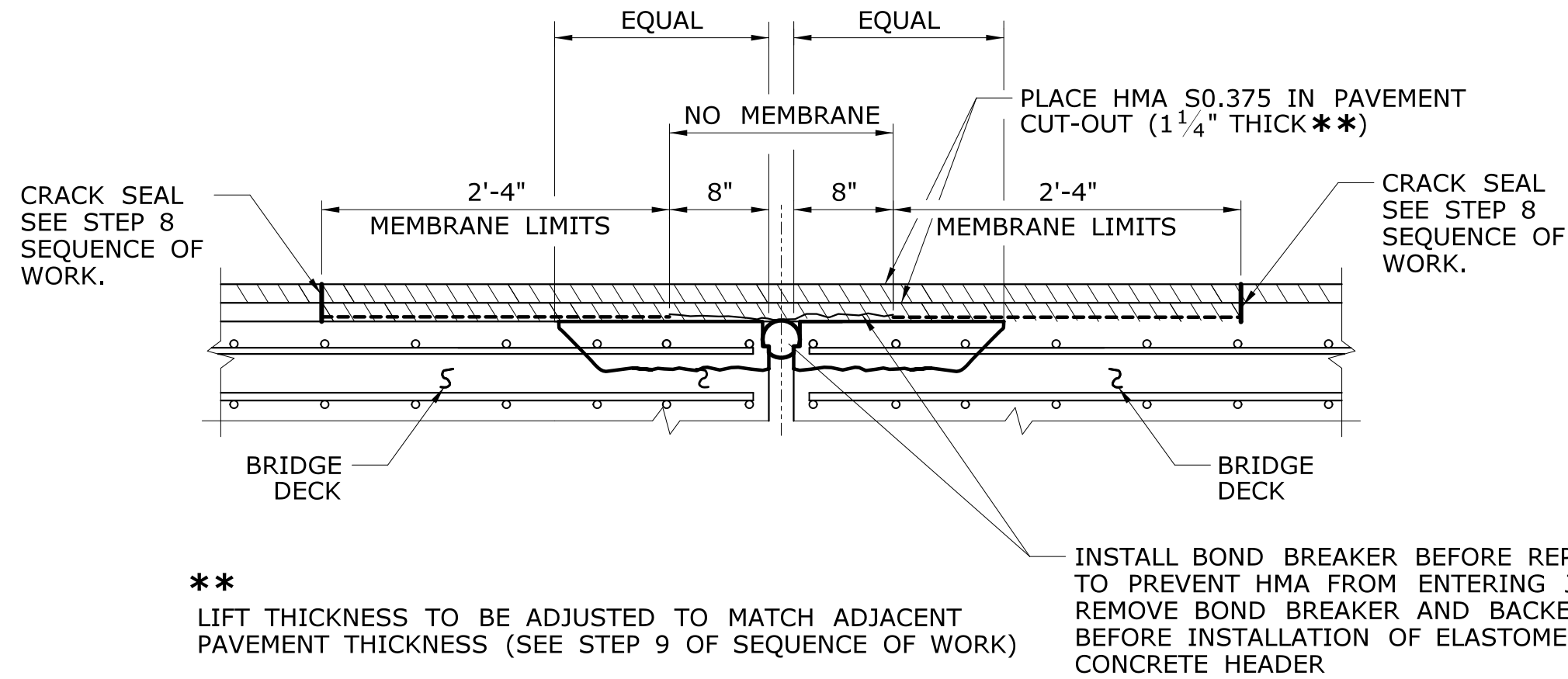
1. REFER TO STEP 1 OF SEQUENCE OF WORK.
2. ALL DIMENSIONS THAT REFER TO EXISTING CONSTRUCTION SHALL BE VERIFIED AT THE SITE BY THE CONTRACTOR.



**INTERMEDIATE CONDITION  
JOINT AND PAVEMENT REMOVAL (STEPS 1-3)**

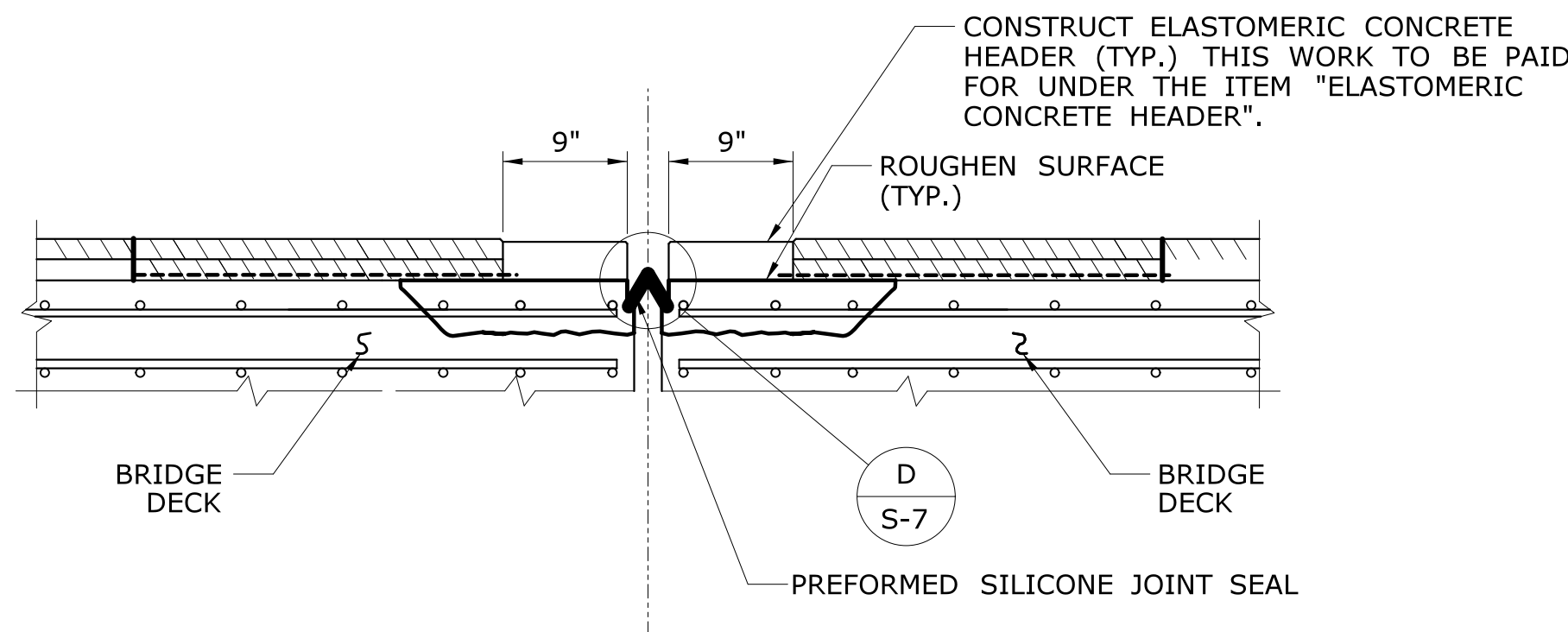


**INTERMEDIATE CONDITION  
(STEPS 4-5)**

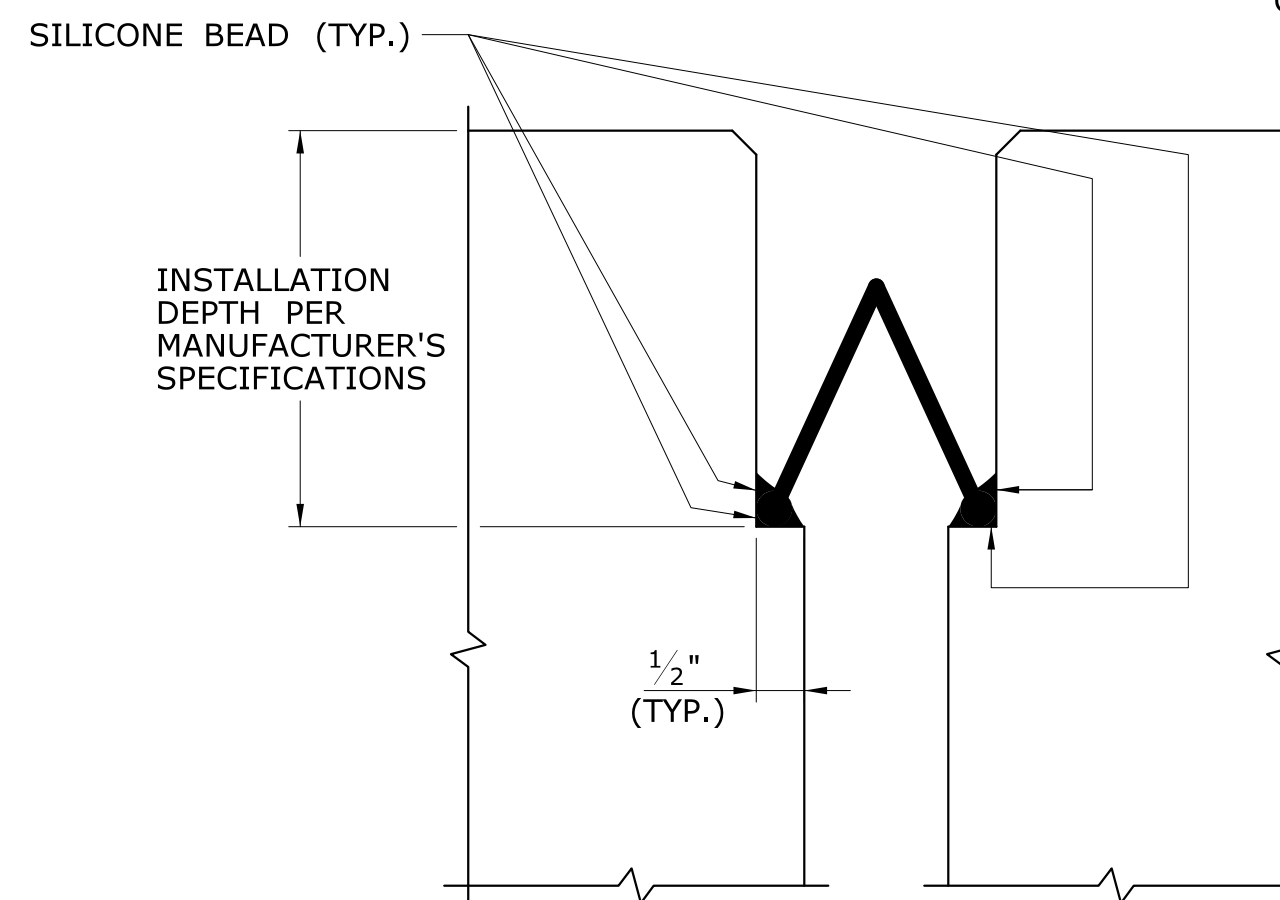


**\*\***  
LIFT THICKNESS TO BE ADJUSTED TO MATCH ADJACENT PAVEMENT THICKNESS (SEE STEP 9 OF SEQUENCE OF WORK)

**INTERMEDIATE CONDITION  
(STEPS 6-9)**



**FINAL CONDITION  
(STEP 10-14)**



**D  
S-7  
PREFORMED SILICONE JOINT SEAL**

NOTE: SEE DRAWING S-9 FOR STAGE CONSTRUCTION JOINT IN PARAPET CUTOUT

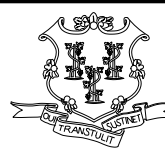
**PROPOSED PREFORMED SILICONE JOINT SEAL  
AT PIER NOS 1 AND 2, BRIDGE NO. 01770**

N.T.S.

**C**

**SUGGESTED SEQUENCE OF WORK**

- STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.
- STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT CUT-OUT. EACH SAW-CUT LINE SHALL BE 3' FROM THE END OF THE DECKS. SAW-CUT SHALL NOT DAMAGE EXISTING DECK.
- STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT MATERIALS (BINDER, BRIDGING PLATE, LOCATING PIN AND BACKER ROD) WITHIN THE LIMITS SHOWN.
- STEP 4: REMOVE SUFFICIENT CONCRETE MATERIAL TO ALLOW FOR 1" BELOW THE REINFORCEMENT AS SHOWN IN THE DETAIL.
- STEP 5: POUR CONCRETE TO THE ELEVATION OF THE BRIDGE DECK. CREATE A STEP AS SHOWN IN DETAIL S-
- STEP 6: INSTALL MEMBRANE TO THE TOP OF DECK WITHIN THE LIMITS SHOWN.
- STEP 7: INSTALL BOND BREAKER AT THE LOCATION OF THE PROPOSED ELASTOMERIC CONCRETE HEADER.
- STEP 8: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES.
- STEP 9: PLACE HMA S0.375 IN THE JOINT CUTOUT. (REFER TO NOTICE TO CONTRACTOR - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS)
- STEP 10: CUT PAVEMENT FULL DEPTH, 9" FROM THE BRIDGE DECK ENDS AS SHOWN IN THE DETAIL. REMOVE ALL PAVEMENT MATERIAL BETWEEN SAW-CUTS AND THEN REMOVE THE BOND BREAKER.
- STEP 11: CLEAN AND RE-SEAL THE JOINT BETWEEN THE DECK ENDS.
- STEP 12: INSTALL THE PROPOSED ELASTOMERIC HEADER AS SHOWN IN THE DETAIL.
- STEP 13: INSTALL THE SILICOFLEX JOINT SEAL.
- STEP 14: APPLY CRACK SEAL TO PAVEMENT SURFACE AT SAW-CUT JOINTS AND AT THE CURB LINE.

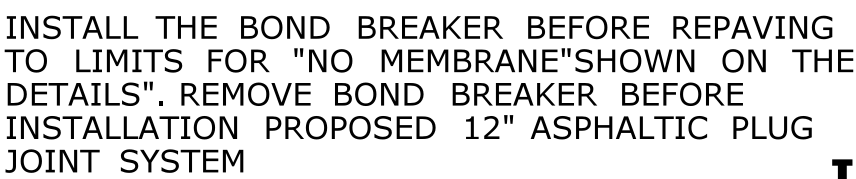
			THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	DESIGNER/DRAFTER: <b>BU</b>		 <b>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</b>	SIGNATURE/ BLOCK: <b>OFFICE OF ENGINEERING</b>	PROJECT TITLE: <b>PAVEMENT PRESERVATION ON I-95</b>	TOWN: <b>TOWN OF GROTON</b>	PROJECT NO. <b>58-329</b>						
				CHECKED BY: <b>RPL</b>						DRAWING NO. <b>S-7</b>						
				SCALE AS NOTED						SHEET NO. <b>04.07</b>						
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/17/2013		Filename: ...\\SB-MSH-S-7_PREFORMED-SILICONE-JOINT-DETAIL.dgn										



1. REFER TO STEP 1 OF SEQUENCE OF WORK.
2. ALL DIMENSIONS THAT REFER TO EXISTING CONSTRUCTION SHALL BE VERIFIED AT THE SITE BY THE CONTRACTOR.



### **INTERMEDIATE CONDITION MILLING AND PAVING (STEPS 7 & 8)**



## INTERMEDIATE CONDITION (STEPS 4-6)

**PROPOSED 12" ASPHALTIC PLUG EXPANSION JOINT SYSTEM  
AT ABUTMENT NOS 1 AND 2, BRIDGE NO. 01770**

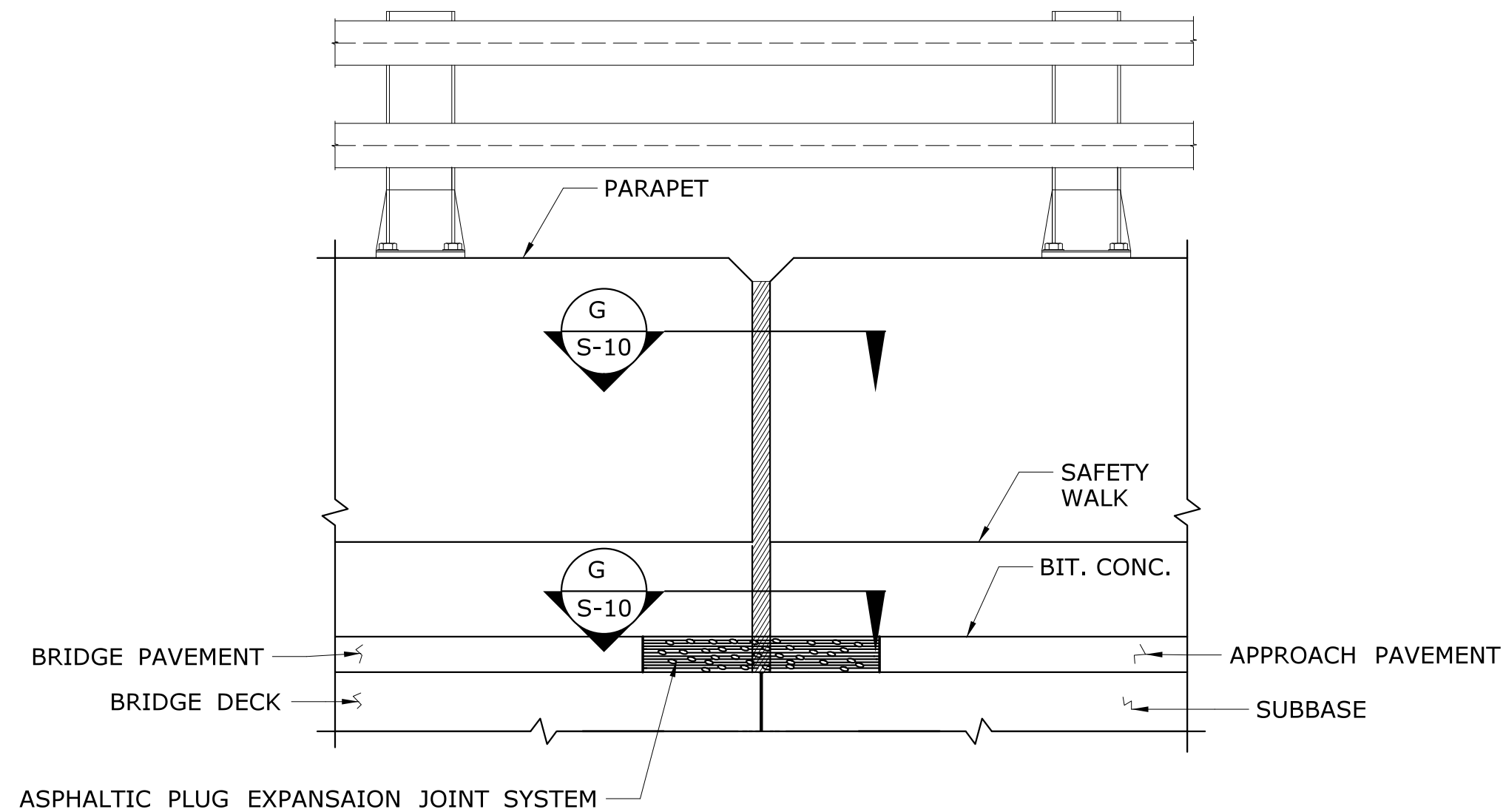
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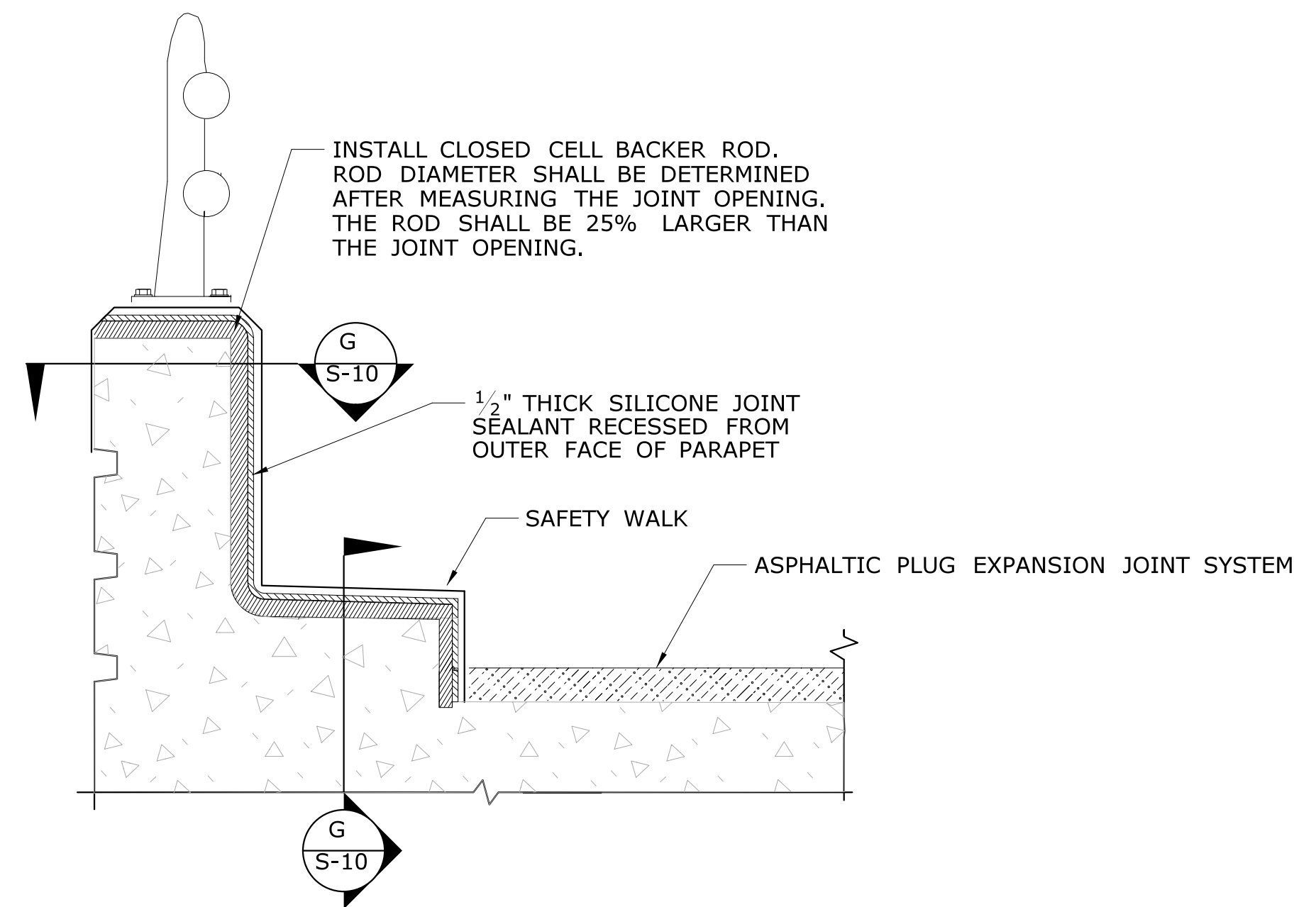
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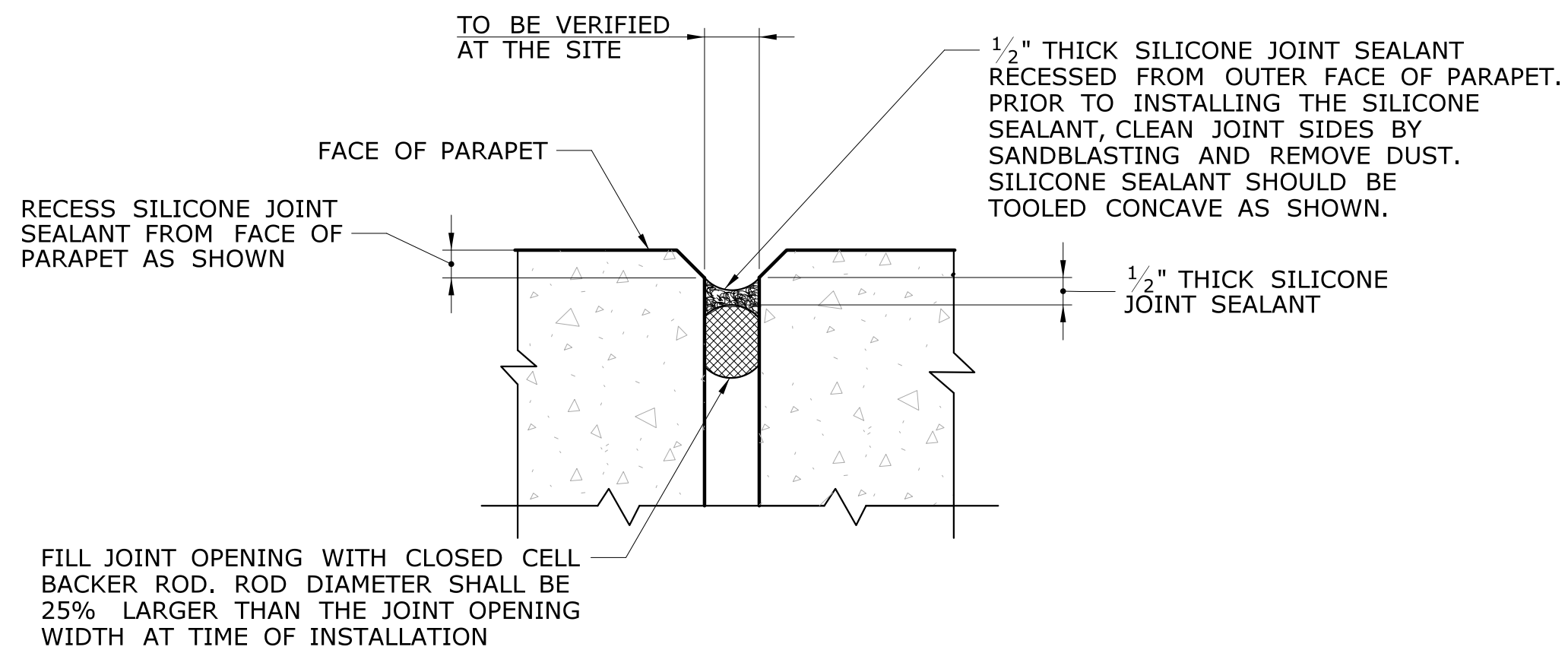


**JOINT TREATMENT AT PARAPET**  
NOT TO SCALE

**NOTE:**  
PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".



**JOINT TREATMENT AT PARAPET**  
NOT TO SCALE





**SECTION**  
NOT TO SCALE

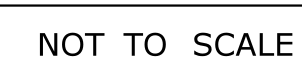
**G**  
**S-10**

**JOINT TREATMENT IN PARAPET AT ABUTMENTS**

NOT TO SCALE

**F**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/17/2013	DESIGNER/DRAFTER: <b>BU</b>	CHECKED BY: <b>RPL</b>	SCALE AS NOTED	 <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b>	SIGNATURE/ BLOCK: <b>OFFICE OF ENGINEERING</b> APPROVED BY: 	PROJECT TITLE:  <b>PAVEMENT PRESERVATION ON I-95</b>	TOWN: <b>TOWN OF GROTON</b>	PROJECT NO. <b>58-329</b>
											DRAWING TITLE: <b>PARAPET JOINT DETAILS 1</b>	DRAWING NO. <b>S-10</b>
												SHEET NO. <b>04.010</b>



**EXISTING**  
NOT TO SCALE

STEP 1: 3/4" SAW-CUT SAFETY CURB AND PARAPET AS SHOWN IN THE DETAILS.

STEP 2: REMOVE ANY ADDITIONAL CONCRETE NECESSARY TO OBTAIN THE REQUIRED REINFORCEMENT CLEARANCES AND CONCRETE COVER SHOWN IN THE DETAILS.

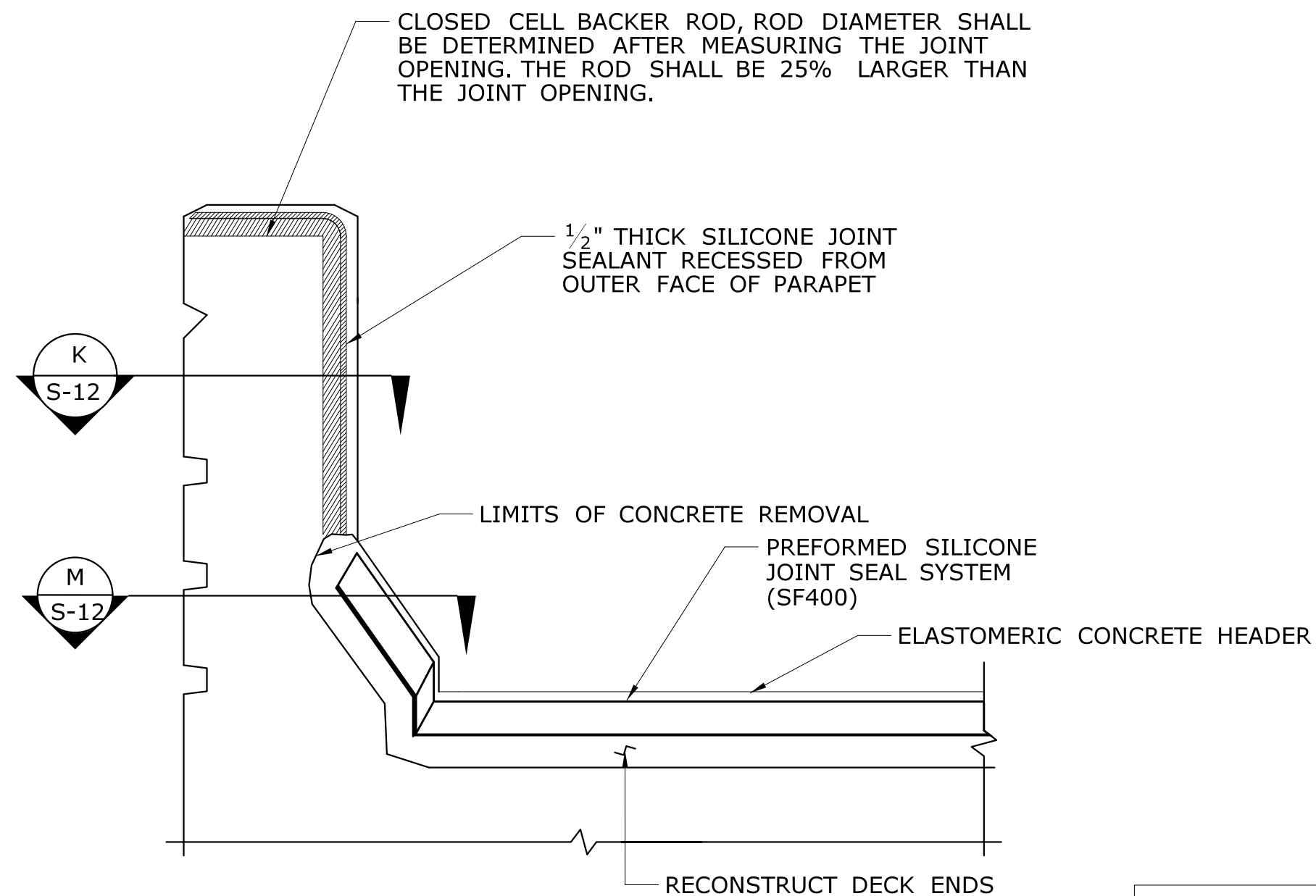
STEP 3: DRILL AND GROUT ANGLE DOWELS INTO THE BRIDGE DECK AS SHOWN IN THE DETAILS. PLACE TRANSVERSE REINFORCEMENT ON GROUTED DOWELS AS SHOWN IN THE DETAILS.  
SEE SECTION 'D' ON DRAWING S-12

STEP 4: PLACE CONCRETE FOR THE PROPOSED CONCRETE SAFETY CURB AND PARAPET. CONSTRUCT THE JOINT IN THE SAFETY CURB AND PARAPET AS SHOWN IN THE DETAIL.



NOT TO SCALE

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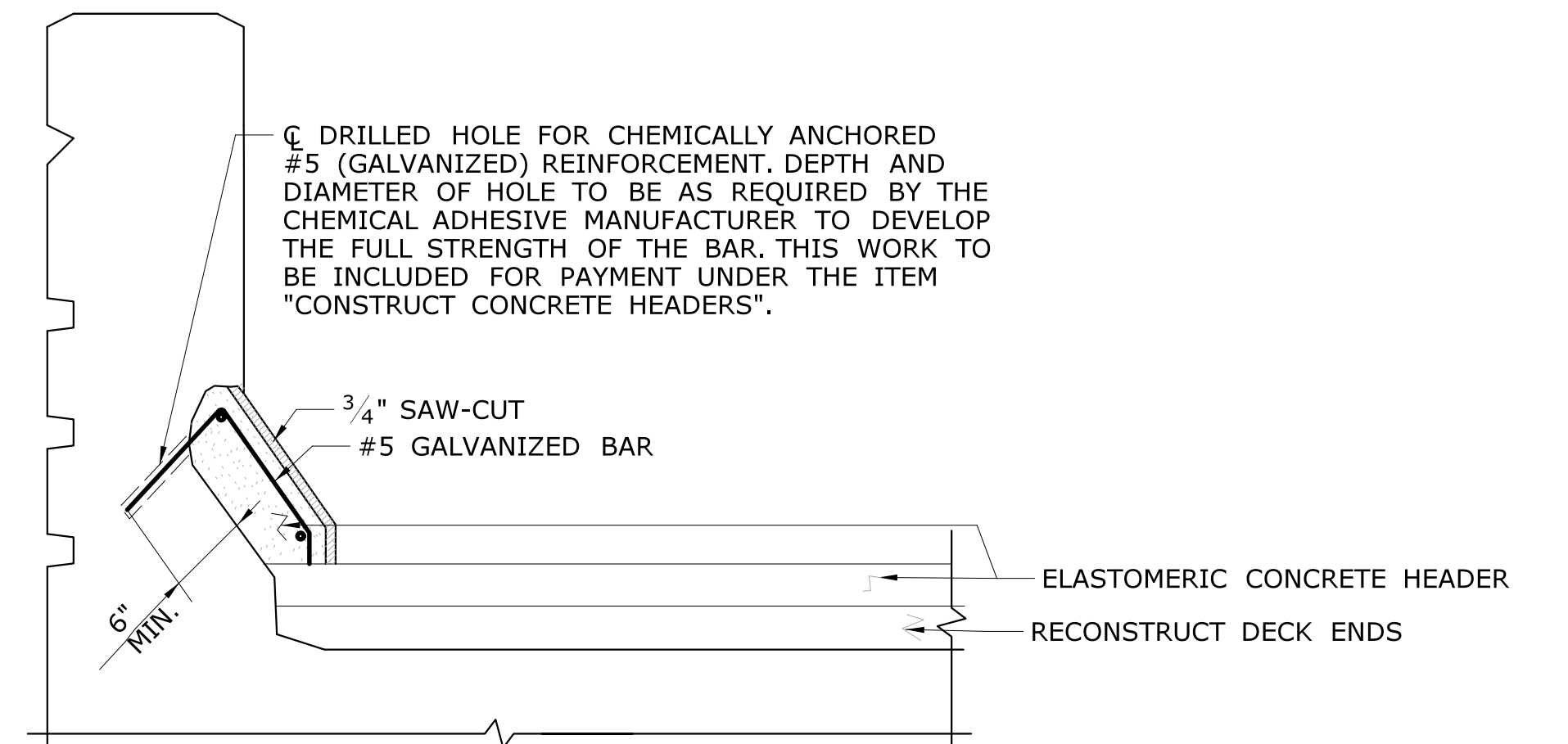


**SECTION - PARAPET**

NOT TO SCALE

**J**  
**S-11**

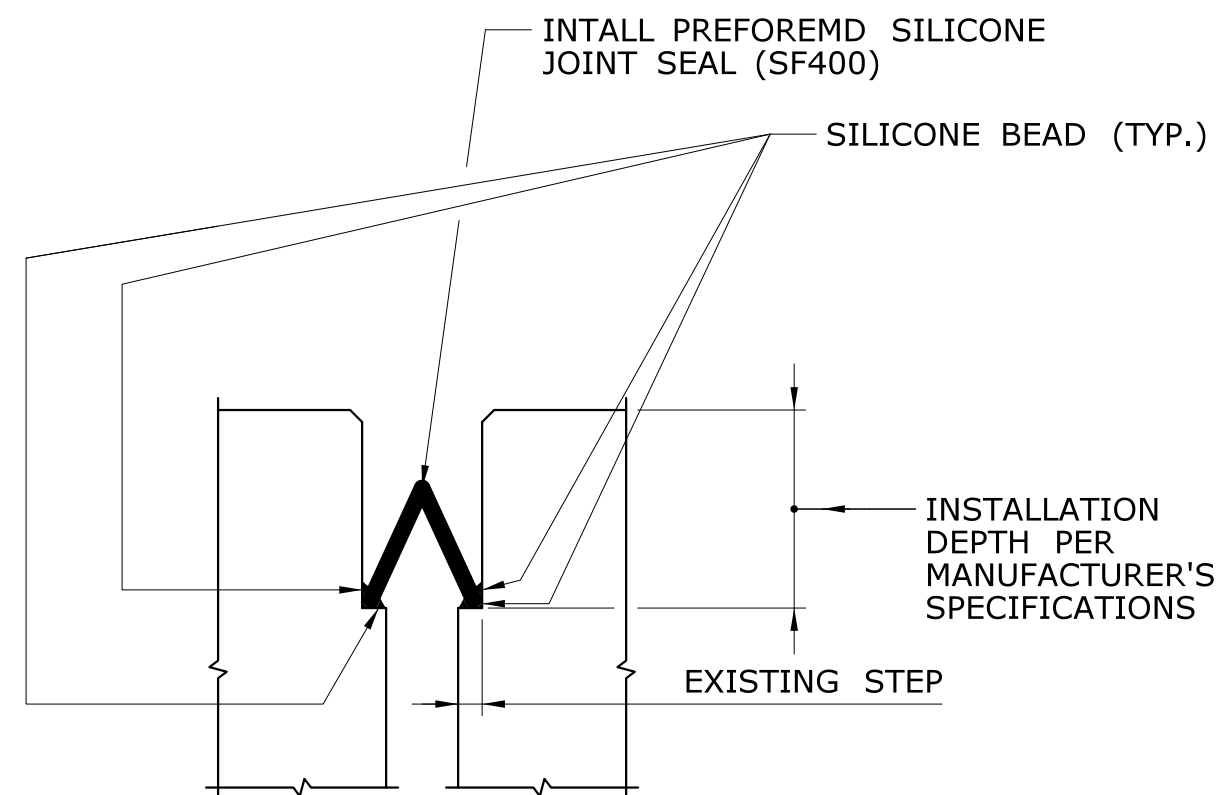
NOTE:  
PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".



**SECTION - PARAPET**

NOT TO SCALE

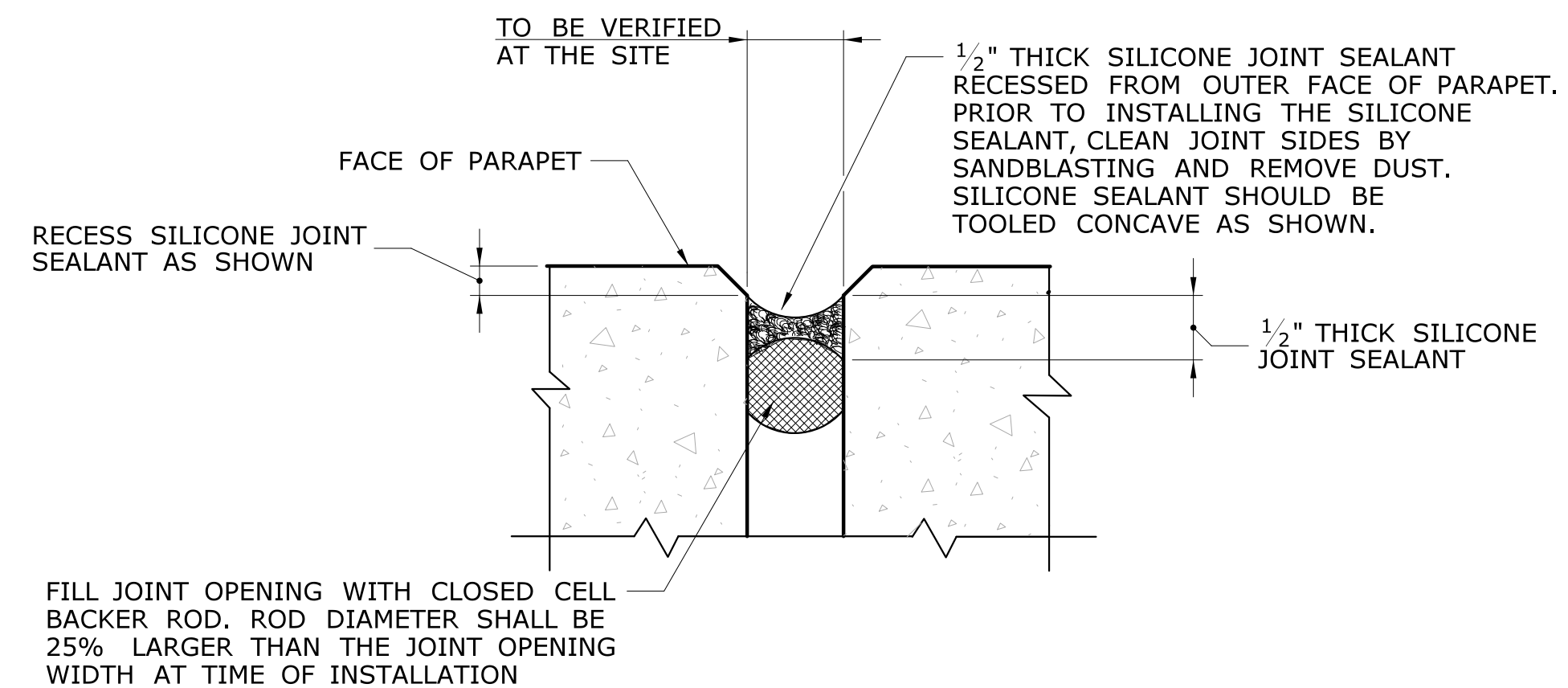
**K**  
**S-11**



**SECTION**

NOT TO SCALE

**M**  
**S-12**



**SECTION THROUGH PARAPET OR MEDIAN BARRIER JOINT**



**LARGE OPENING**

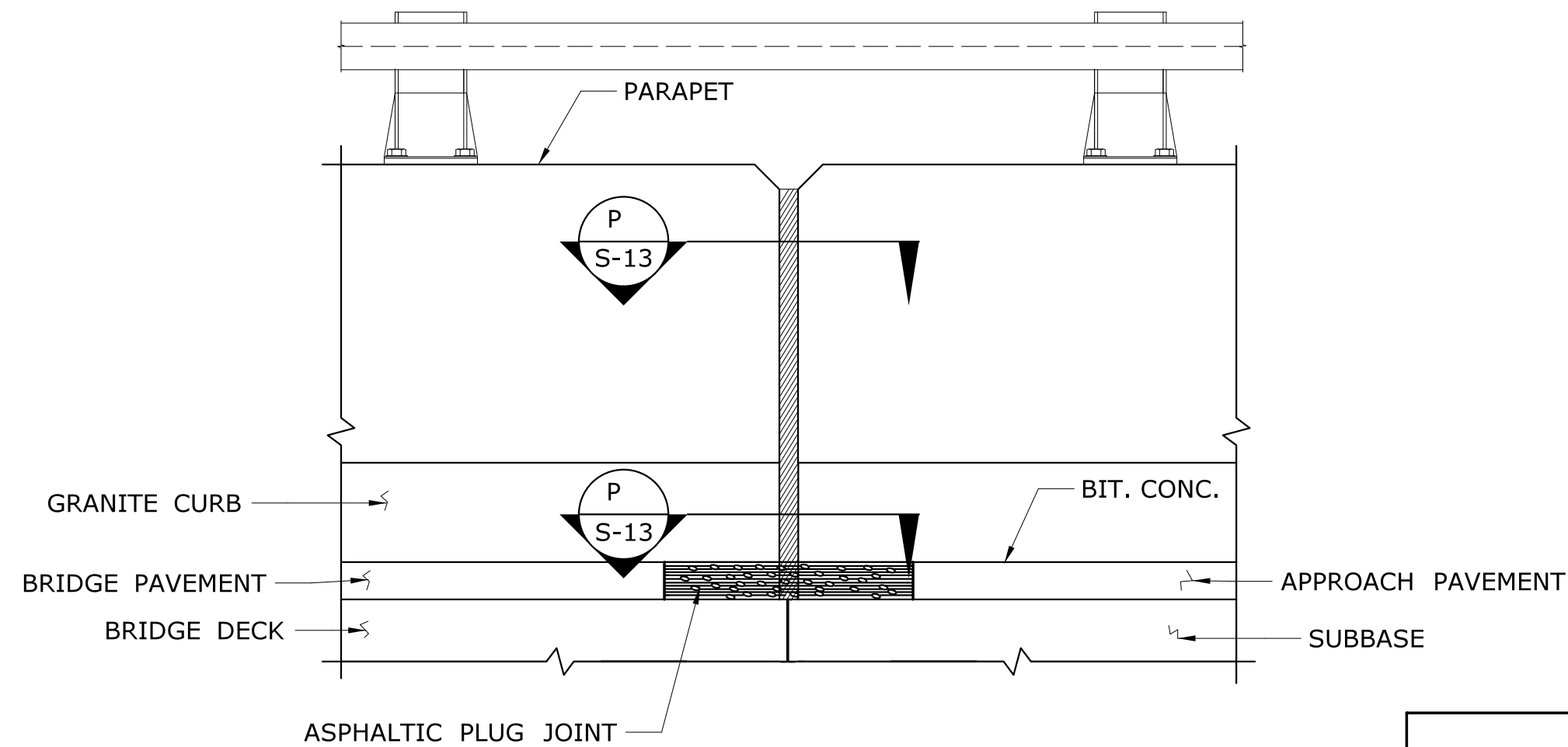
NOT TO SCALE

**N**  
**S-11**

**JOINT TREATMENT IN PARAPET AT PIER NOS. 1 AND 2 ,BR. NO. 01770**

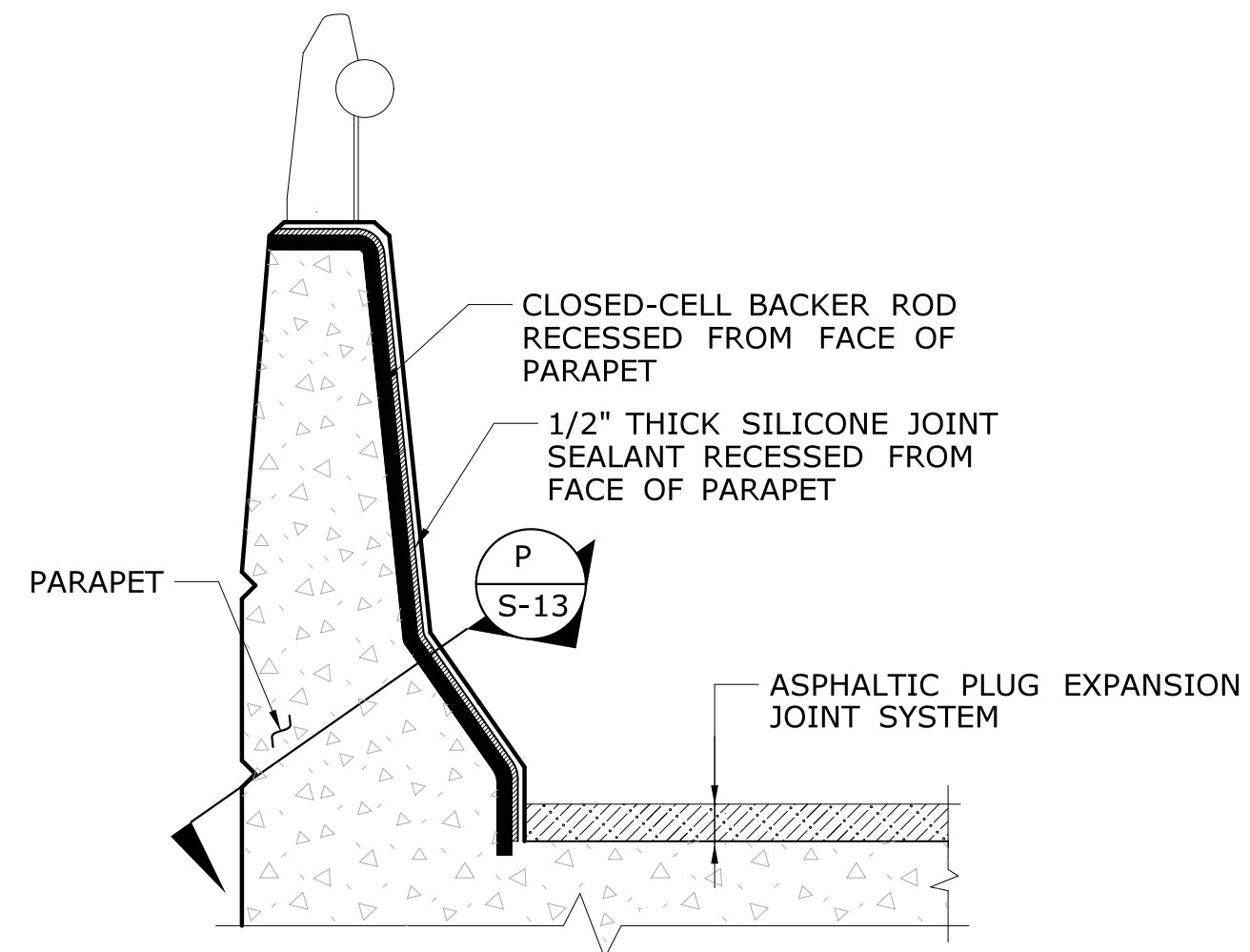
NOT TO SCALE

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/17/2013	DESIGNER/DRAFTER: <b>BU</b>	CHECKED BY: <b>RPL</b>	SCALE AS NOTED	 <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b> Filename: ...\\SB_MSH_S-12_PARAPET_JOINT_DETAIL_2B.dgn	SIGNATURE/ BLOCK: <b>OFFICE OF ENGINEERING</b> APPROVED BY: 	PROJECT TITLE: <b>PAVEMENT PRESERVATION ON I-95</b>	TOWN: <b>TOWN OF GROTON</b>	PROJECT NO. <b>58-329</b>
											DRAWING NO. <b>S-12</b>	SHEET NO. <b>04.012</b>
											<b>PARAPET JOINT DETAILS 2B</b>	

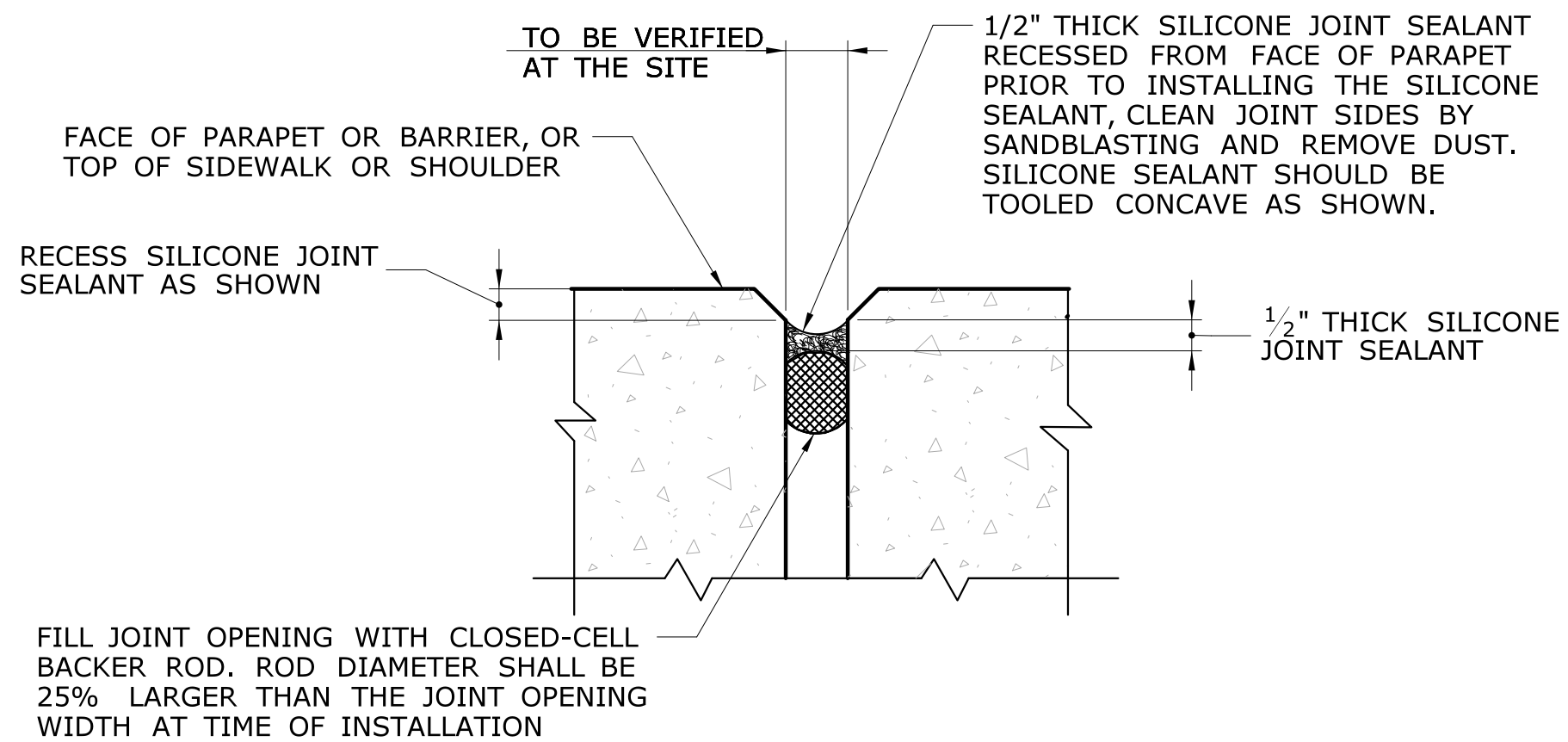


**JOINT TREATMENT AT PARAPET**  
NOT TO SCALE

**NOTE:**  
PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".



**JOINT TREATMENT AT PARAPET**  
NOT TO SCALE



**SECTION THROUGH PARAPET JOINT** **P S-13**  
NOT TO SCALE

**SILICONE JOINT SEALANT AND BACKER ROD DETAILS AT PARAPET AND MEDIAN BARRIER, BR. NO. 02445**  
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Diagram illustrating the cross-section of a median barrier expansion joint system. The diagram shows a concrete barrier structure with a longitudinal joint. Key components and labels include:

- CENTERLINE OF MEDIAN
- LONGITUDINAL JOINT (NOT TYPICAL)
- CLOSED-CELL BACKER ROD RECESSED FROM FACE OF BARRIER
- 1/2" THICK SILICONE JOINT SEALANT RECESSED FROM FACE OF BARRIER
- R S-14 (Reference to a standard or specification)
- MEDIAN BARRIER
- ASPHALTIC PLUG EXPANSION JOINT SYSTEM
- BACKER ROD WHERE STEEL BRIDGING PLATE IS USED
- EXTEND BACKER ROD FOR ASPHALTIC PLUG EXPANSION JOINT SYSTEM BEYOND GUTTER.



## JOINT TREATMENT AT CONCRETE MEDIAN BARRIER AT BRIDGE NO. 01639 - SOUTHBOUND

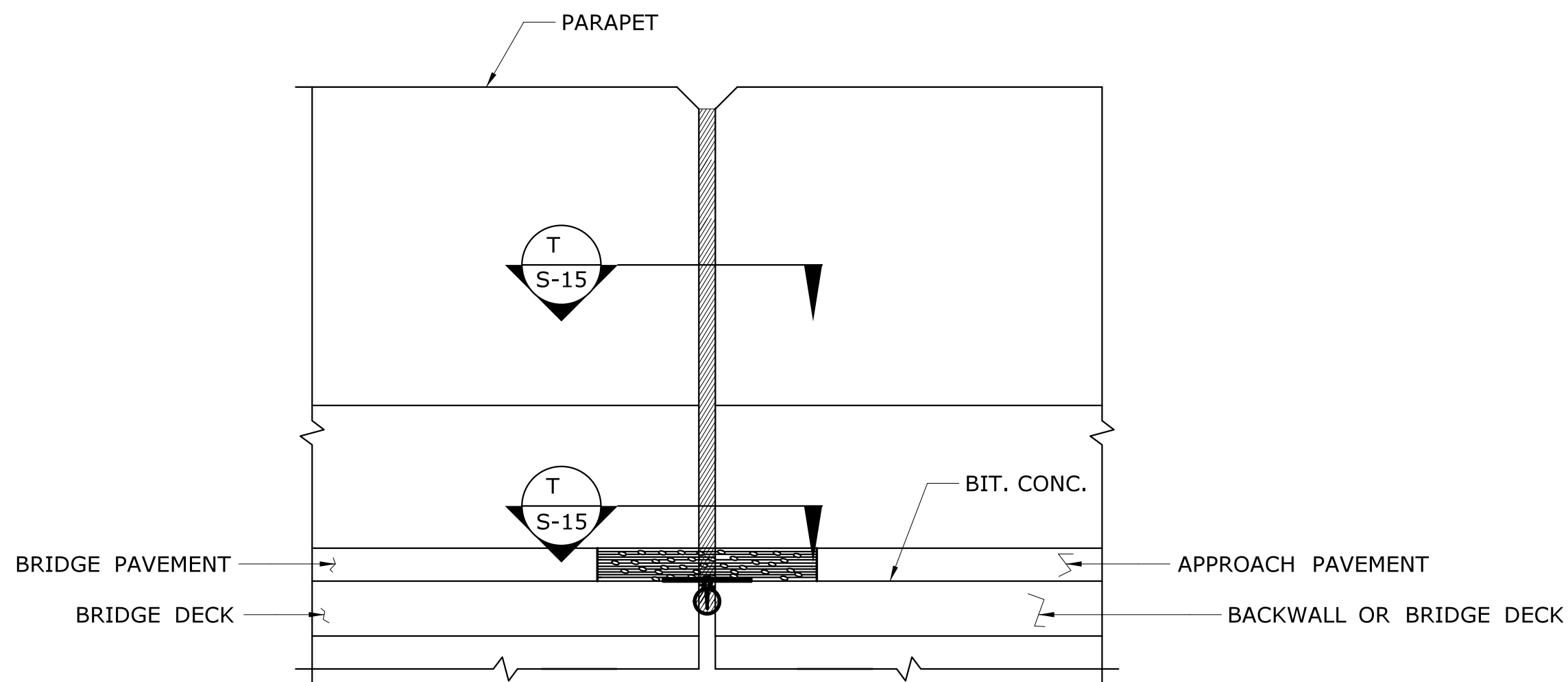
NOT TO SCALE



**SILICONE JOINT SEALANT AND BACKER ROD DETAILS  
AT PARAPET AND MEDIAN BARRIER, SOUTHBOUND - BR. 01639**

NOT TO SCALE

-	-	-	-	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	DESIGNER/DRAFTER: <b>BU</b>	 <b>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</b>	SIGNATURE/ BLOCK:	PROJECT TITLE:  <b>TOWN OF GROTON PAVEMENT PRESERVATION ON I-95</b>	TOWN:  <b>TOWN OF GROTON</b>	PROJECT NO. <b>58-329</b>
-	-	-	-		CHECKED BY: <b>RPL</b>		OFFICE OF ENGINEERING			DRAWING NO. <b>S-14</b>
-	-	-	-				APPROVED BY: 			SHEET NO. <b>04.014</b>
-	-	-	-		SCALE AS NOTED					
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REV.	DATE	REVISION	DESCRIPTION	SHEET NO.	Plotted Date: 10/17/2013	Filename: ...\\SB-MSH-S-14-PARAPET_JOINT DETAIL-4.dgn				



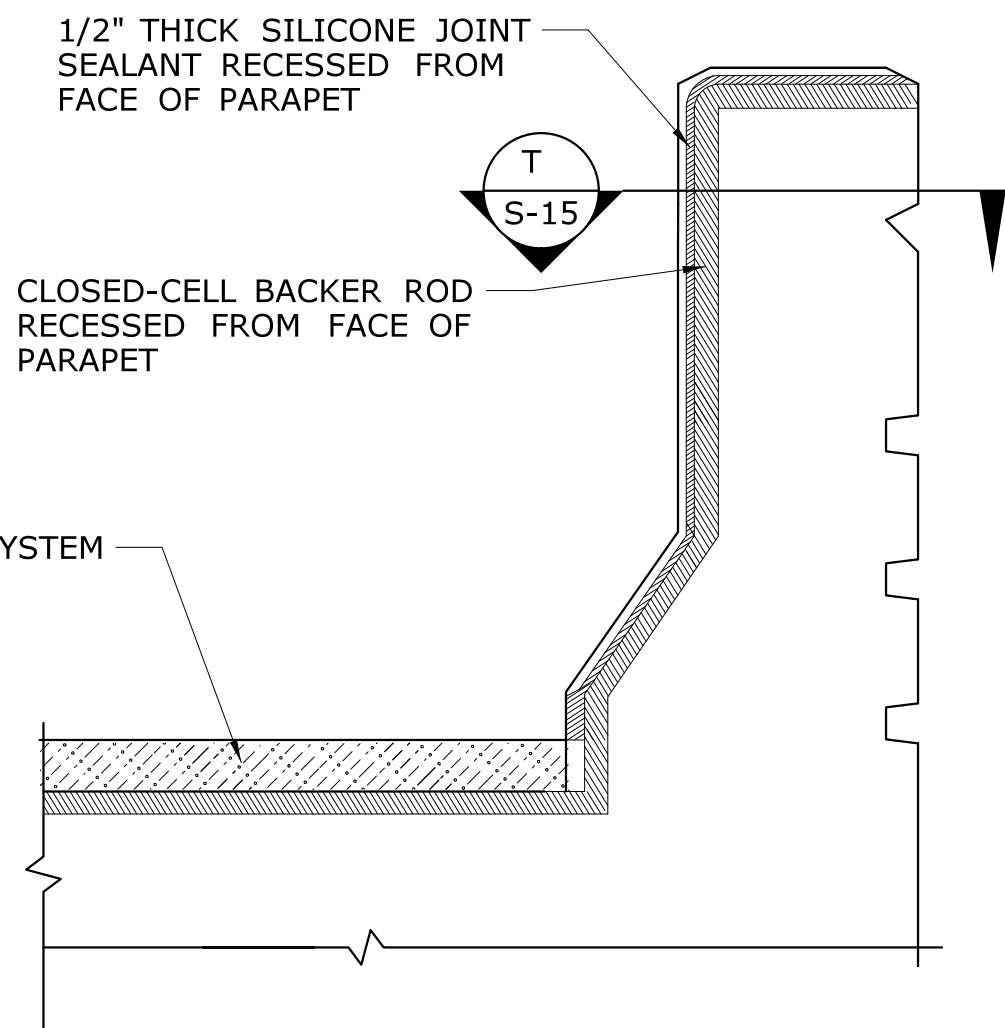
### JOINT TREATMENT AT PARAPET

NOT TO SCALE

#### NOTE:

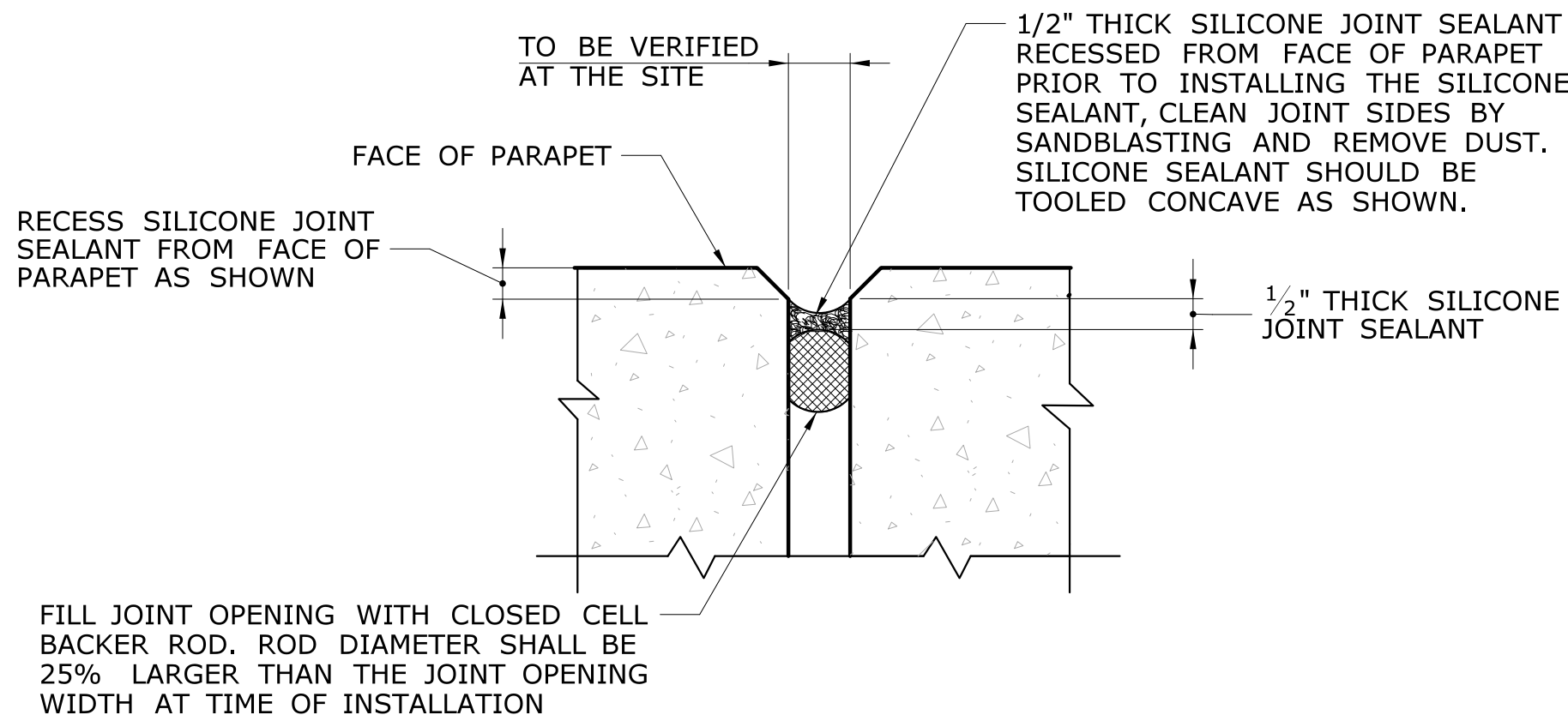
PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".

ASPHALTIC PLUG EXPANSION JOINT SYSTEM



### K SECTION - PARAPET

NOT TO SCALE





### SECTION THROUGH PARAPET OR MEDIAN BARRIER JOINT

NOT TO SCALE

### SILICONE JOINT SEALANT AND BACKER ROD DETAILS IN PARAPET

NOT TO SCALE

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/17/2013	DESIGNER/DRAFTER: <b>BU</b>	CHECKED BY: <b>RPL</b>	SCALE AS NOTED	 <b>STATE OF CONNECTICUT</b> <b>DEPARTMENT OF TRANSPORTATION</b>	SIGNATURE/ BLOCK: <b>OFFICE OF ENGINEERING</b> APPROVED BY: 	PROJECT TITLE:  <b>PAVEMENT PRESERVATION ON I-95</b>	TOWN:  <b>TOWN OF GROTON</b>	PROJECT NO. <b>58-329</b>
											DRAWING TITLE: <b>PARAPET JOINT DETAILS 5</b>	DRAWING NO. <b>S-15</b>
												SHEET NO. <b>04.015</b>